

CITY COUNCIL COMMITTEE

EARLY CALIFORNIA ARCHITECTURE

Tim Grayson, Chair
Ron Leone, Committee Member
Kirk Shelby, Committee Member

5:30 p.m.
Monday, February 8, 2016

Bldg. D, Permit Center Conference Room
1950 Parkside Drive, Concord

- A G E N D A -

ROLL CALL

PUBLIC COMMENT PERIOD

1. **REVIEW** – of Todos Santos Design Guidelines – 30% Progress Review Draft. Report by Joan Ryan, Senior Planner, and Afshan Hamid, Associate Planner.
2. **ADJOURNMENT**

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**REPORT TO THE EARLY CALIFORNIA ARCHITECTURAL
REVIEW COMMITTEE/DESIGN REVIEW BOARD****TO HONORABLE COMMITTEE/DRB MEMBERS:**

DATE: February 8, 2016

**SUBJECT: REVIEW OF TODOS SANTOS DESIGN GUIDELINES –
30% PROGRESS REVIEW DRAFT****Report in Brief**

On November 16, 2015, Planning Division staff initiated the preparation of the Todos Santos Design Guidelines (Guidelines) based on the direction from the Housing and Economic Development Committee. Staff has scheduled a “milestone review” with the Early California Architectural Review (ECAR) Committee and the Design Review Board (DRB) at 30% completion of the project. Staff intends to present the 30% “progress review draft” to the City Council at a study session in March.

Attachment 1 represents the 30% “progress review draft” for the ECAR Committee’s review and feedback. Staff is soliciting the ECAR Committee’s and DRB’s comments on the entire document and requests specific feedback on the following:

- Vision and Policy Statement in Chapter 1;
- Building Form in Chapter 2;
- Inner and outer core elements on pages 27-30 in Chapter 2; and
- Fenestration and the photographic examples provided in Chapter 3.

Background

On November 16, 2015, the Housing and Economic Development (HED) Committee met to discuss the preparation of the Guidelines for the downtown. The HED Committee accepted the recommended boundaries that will be used to define “inner core” and “outer core” downtown areas where the Guidelines will be in effect (see pages 27-30 of the Guidelines). The inner core Guidelines encourages architectural designs that incorporate elements of early California architectural style, while the outer core Guidelines allow early California inspired architectural themes as well as more flexible, contemporary architectural styles. The HED Committee considered thresholds to determine when new buildings and/or building renovations would be subject to the Guidelines. The HED Committee agreed that projects with exterior facade improvements of \$50,000 or greater would be required to comply with the Guidelines. However, staff is interested in further discussion of such thresholds, with consideration given to the type of work being proposed, such as a change in signage, a façade improvement, or new construction and the location of improvements, for example, those directly facing Todos Santos Plaza.

In December 2015, the DRB requested a joint meeting with the ECAR Committee to ensure consistency in the policy direction for the Todos Santos Guidelines. This staff report was prepared for the February 8 joint meeting between the DRB and the ECAR Committee to discuss the 30% progress review draft of the Todos Santos Design Guidelines.

REVIEW OF TODOS SANTOS DESIGN GUIDELINES- 30% PROGRESS

February 8, 2016

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Discussion

The 30% progress version of the Guidelines sets the framework for the remainder of the document. A Table of Contents is included and the first three chapters have been drafted. These include:

Chapter 1 - Introduction: Including the background, the intent and purpose of the document, a policy and vision statement, community outreach, an architectural overview, and a summary of the architectural influences in Concord. Much of the direction is based upon the stated goals and objectives in the Downtown Specific Plan.

Chapter 2 - Building Form: Including the developable building area defined by the density, floor area ratio, setbacks, lot coverage and height restriction requirements of the Development Code. A discussion is included of building form, site planning and orientation, massing, scale and proportion, and photo examples of local multi-family design elements. A table of inner core vs. outer core elements is summarized. Architectural Guidelines for the inner core focus on those elements consistent with the Spanish Revival, Mission, Monterey Colonial, Arts and Crafts and Victorian architectural styles, reminiscent of early California. Outer core focuses on early California inspired architectural themes as well as more flexible, contemporary architectural styles and elements that include form, materials, detail and a range of amenities. Photographic examples are provided of some architectural styles that would be appropriate to the inner core and the outer core areas.

Chapter 3 - Fenestration: Discussing the arrangement of windows and doors on the elevation of a building. A building's fenestration plays an important role in making the building people-oriented and relating well to the street.

The next 60% progress draft will include Chapters: 4 - Roof Form; 5 - Materials and Finishes; 6 - Public vs. Private Realm; and 7 - Details. The 90% progress draft will include Chapters: 8 - Landscaping; 9 - Signage; 10 - Review Procedures and Checklist; 11 - References; and an Appendix detailing further photographic documentation of early Concord structures and other historical images.

Next Steps

The work program began in September with the first HED Committee meeting, and two follow up meetings. Staff intends to present the 30% progress review draft of the Guidelines, together with the ECAR Committee/DRB comments, to the City Council in a study session on March 21. Staff will then return to the DRB with the 60% progress draft that has incorporated all comments from this meeting and the City Council, and build upon the current draft with further detail. The following table reflects those meetings as well as the draft work program for the completion of the Todos Santos Design Guidelines.

Date	Body	Body Members	Subject	Outcome (achieved or anticipated)
<i>Sept 28, 2015</i>	<i>Housing & Econ. Development</i>	<i>Helix, Chair; Leone</i>	<i>Background and geographic area of application</i>	<i>Study Session</i>
<i>Oct 26, 2015</i>	<i>Housing & Econ. Development</i>	<i>Helix, Chair; Leone</i>	<i>Geographic area of application</i>	<i>Study Session</i>
<i>Nov 16, 2015</i>	<i>Housing & Econ. Development</i>	<i>Grayson, Chair; Leone</i>	<i>Geographic area of application</i>	<i>Establishing Inner and Outer Core</i>

REVIEW OF TODOS SANTOS DESIGN GUIDELINES- 30% PROGRESS

February 8, 2016

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Date	Body	Body Members	Subject	Outcome (achieved or anticipated)
Feb 8, 2016	ECAR Com. & DRB	Leone, Chair; Grayson; DRB Members	Review 30% Guidelines	Study Session
March 21	City Council	Full Council	Review 30% Guidelines	Study session
April 7	DRB meeting	DRB Members	Review 60% Guidelines	Study session
May 19	DRB meeting	DRB	Review 100% Guidelines	Final review
May 23	HED Committee	<i>Helix, Chair;</i> <i>Leone</i>	Review 100% Guidelines	Approval to forward to PC
June 1	Planning Commission	PC Members	Review 100% T Guidelines	Recommendation to forward to Council
July 12	City Council	Full Council	Review 100% Guidelines	Final approval

Public Contact

The agenda item was posted. Developers with an interest in Concord's downtown have been notified.

Staff Recommendation

Staff is soliciting the comments of the ECAR Committee and the DRB on the entire document and requests specific feedback on the following:

- Vision and Policy Statement in Chapter 1;
- Building Form in Chapter 2;
- Inner and outer core elements on pages 27-30 in Chapter 2; and
- Fenestration and the photographic examples provided in Chapter 3.

In addition, staff is interested in further discussion of thresholds, with consideration given to the type of work being proposed, such as a change in signage, a façade improvement, or new construction and the location of improvements, for example, those directly facing Todos Santos Plaza.

REVIEW OF TODOS SANTOS DESIGN GUIDELINES- 30% PROGRESS

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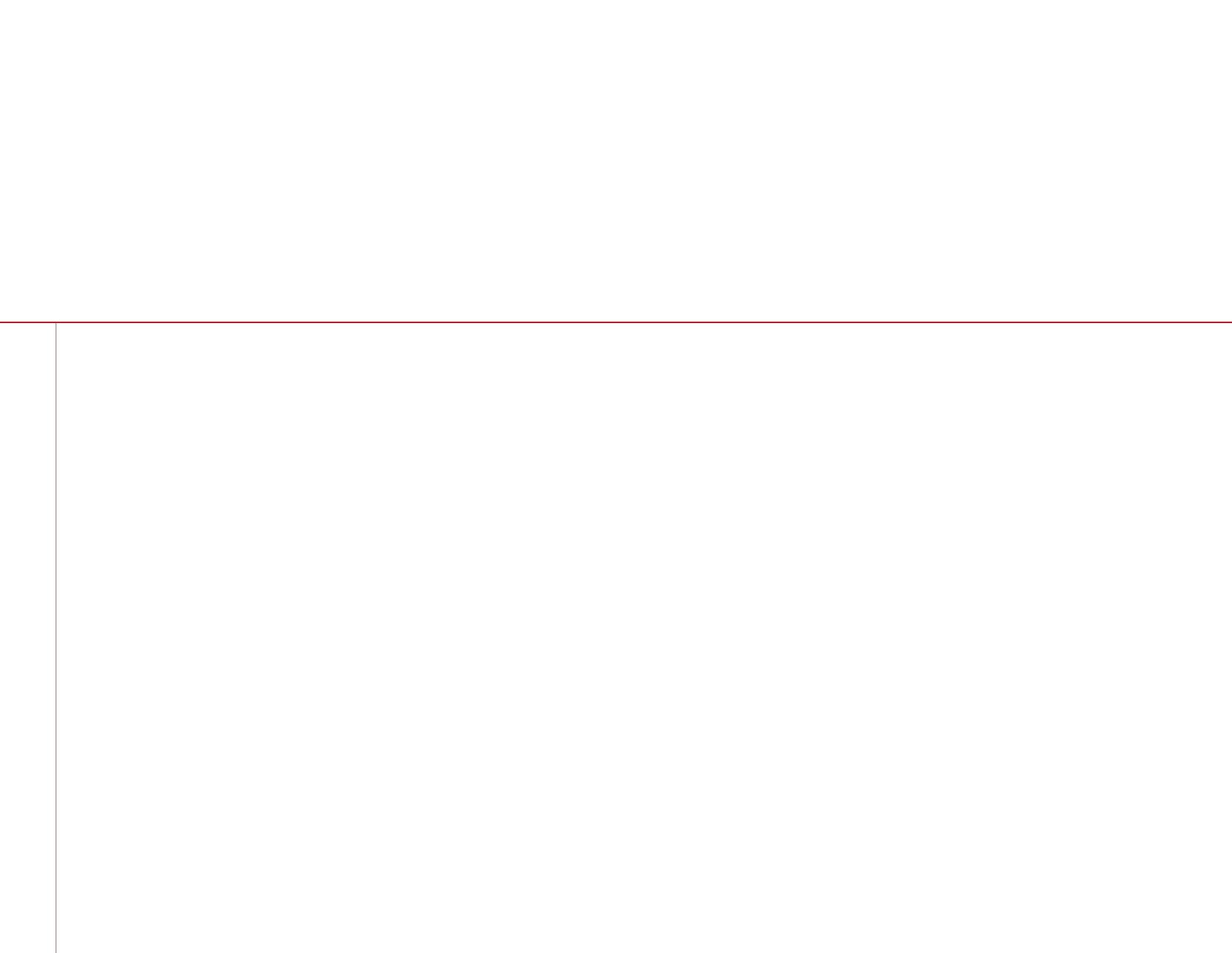
Attachment 1 – Todos Santos Design Guidelines (30% Progress Review Draft)



City of Concord, Ca

30% DRAFT: Todos Santos Design Guidelines

Adopted : July 2016 by the City of Concord Planning Commission



Todos Santos Design Guidelines





Acknowledgements

City of Concord, California
First Edition, Adopted July XX, 2016
Community & Economic Development Department

Mayor & City Council:

Mayor Laura M. Hoffmeister
Vice Mayor Ron Leone
Councilmember Edi E. Birsan
Councilmember Tim Grayson
Councilmember Daniel C. Helix

Staff Members:

insert here

Ad-Hoc Committee:

Chairman Tim Grayson
Chairman Ron Leone
Chairman Jack Moore, Design Review Board
Board Member Ross Wells, Design Review Board
Board Member Kirk Shelby, Design Review Board
Board Member Peter Harmon, Design Review Board
Board Member Ernesto Avila, Planning Commission Liaison



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The City Council unanimously adopted the Downtown Concord Specific Plan on June 25, 2014, which includes a chapter on Design Guidelines that focuses on urban form, massing and character, ground floor treatment, relating facades to the public realm, vehicular access and building design including parking, servicing and private open space. The guidelines state that the objective is to reflect early California architecture in the design of new buildings. The guidelines expand that into a major policy and state new development to provide consistency in character that enhances and reinforces the existing character of the Downtown. The Design Guidelines when describing Early California Architecture are general in nature and not overly prescriptive. The implementation action outline two measures; A) hold study session with Design Review Board to explore Early California theme B) Prepare Design Guidelines handout for Developers (excerpt from SP).

On September 28, 2015, the Housing and Economic Development Committee reviewed the background of the Downtown Specific Plan, a summary of the Plan's design guidelines, the purpose of the Plan's implementation strategy to further explore the Early California theme, and a presentation on a brief history of Early California architecture. In summary, Early California is a broad term that describes varied architectural styles and influences within California at the turn of the 20th century. These influences include, but are not limited to Spanish Revival,

Arts & Crafts, Monterey Colonial, Victorian, Bungalow, Prairie Style, French Eclectic, Mission Revival architectural styles, all of which have representations within Downtown Concord and the North Todos Santos District.

On October 26, 2015, the Housing & Economic Development Committee expressed a desire to expand the Downtown Concord Specific Plan section on design guidelines and to create new Todos Santos Design Guidelines which will be more architectural.

The Housing & Economic Development Committee described the downtown area as a jewel where they would like to see the past reflected. Developers, architects and property owner comments were generally supportive of the idea of new development incorporating an Early California theme. However, they also provided input stating their need for design flexibility, and a desire for variety to provide attractive curb appeal and address the needs and desires of their tenants in a more contemporary architectural language. The stakeholders emphasized the need for cost sensitivity, and stated they would like the City to provide incentives, and requested the need for a clear process for review of projects. It was suggested that new architectural requirements of Early California architectural themes be more focused in the central area, an inner core surrounding Todos Santos Plaza, with less

prescriptive guidelines away from the central area, an outer core.

The map of the effective areas where the Todos Santos Design Guidelines would be applicable was developed and refined by the Housing & Economic Development Committee. The map is presented on pages 15 and 16. The Inner Core would establish architectural design standards that are more reflective of varied Early California Architecture, while the Outer Core on the map would allow for an architecture that is reflective of quality contemporary architecture.

The Housing & Economic Development Committee provided direction as follows:

- 1) The need to establish boundaries where Early California Architecture would be required, with the immediate area surrounding Todos Santos Plaza and its main street corridors as the central focus, including a hierarchy of guidelines for the inner vs. outer core;
- 2) The need for flexibility in implementation of design guidelines, respecting a variety of architectural styles, and clarifying the process for review;
- 3) The need to respect cost considerations, and seeking some type of incentive program;
- 4) The need for sketches as well as photos in the development of the design standards, to encourage design creativity; and
- 5) The desire to create a transit corridor between BART and Todos Santos Plaza to improve traffic connectivity, with a tram or similar facility.

01

Intent & Purpose

(INTENT & PURPOSE: WHY AND WHERE WE WANT TO GO OVER THE NEXT NUMBER OF YEARS)

The Downtown Concord Specific Plan was adopted on June 6, 2014. Report section 4 of the Specific Plan relates to Design Guidelines. The general intent of the section is to provide a set of building development design guidelines to provide guidance to potential property owners, developers, and the City's design review / staff for determining the architectural character and building development for proposed project. The chapter describes building character, how buildings should relate to the public realm, and address parking and servicing to ensure the strong pedestrian character of the Downtown and area around Todos Santos Plaza is maintained. The Guidelines are general in nature and describe good practices such as Building Setback, Ground Floor Treatment, Residential Ground Floor, Blank Wall Treatment, Vehicular Access, Residential/Mixed Use Building Design, Commercial Building Design, Parking and Servicing, Private Open Space, Sustainable Practices as well as Landscaping. The guidelines illustrate through historic and contemporary examples across northern and southern California how cities promote a quality downtown environment. The examples are focused on achieving walkability, good scale and proportion, creating good neighborhoods and having inviting ground floor spaces.

The purpose of these Todos Santos Design Guidelines (TSDG) will be to expand the

Specific Plan section on Design Guidelines as it relates to downtown Concord. The Downtown Specific Plan states that new buildings demonstrate their respect for the City's heritage through modern interpretations of early California architecture. Thoughtfully blending the old with the new, Downtown Concord is constantly evolving and growing in an organic and sustainable manner.

Purpose: To create guidelines that support authenticity & flexibility, yet respect Concord's historical context.

The intent is that the TSDG guidelines would provide flexibility while at the same time provide further clarity as to the architectural character that resonates with Concord's historical context and guidance on new development. The Design Guidelines are intended to promote quality in design character so that buildings appear authentic. Similar to the Downtown Concord Specific Plan, sketches and photographs are meant only to relay particular concepts in the text or make reference to pertinent precedents and should not be considered exact models. Each development block and new development should be considered unique and evaluated on how well it relates to the overall downtown context. The guidelines will provide varied architectural styles describe the desired overall aesthetic and look for downtown Concord. The architectural design guidelines will serve as a tool to assist

New buildings demonstrate their respect for the City's heritage through modern interpretations of early California architecture. Thoughtfully blending the old with the new, Downtown Concord is constantly evolving and growing in an organic and sustainable manner."

Page 17, Downtown Concord Vision, Downtown Concord Specific Plan

01

Policy Statements

developers and architects in the design of their projects to quickly navigate the design review process.

The Housing & Economic Development Committee, Design Review Board and Stakeholders provided an architectural vision for the downtown:

(VISION STATEMENT : A CONCISE ONE TO TWO SENTENCES WITH BIG PICTURE OF OUR VISION FOR CONCORD)

01 | Community Outreach

At the Housing and Economic Development Committee meeting on September 28, 2015, the Committee requested input from the public, particularly developers in crafting guidelines or standards and invited their comments. Community outreach was conducted by inviting stakeholders as part of the public process. Members of the public provided input and emphasized need for flexibility, and the desire for variety to provide attractive curb appeal. The stakeholders stated:

- The need for flexibility in implementation of design guidelines,
- Respecting a variety of styles,
- Clarifying the process for review.

Developers, property owners and architects were invited to each of the meetings to help shape the overall development and architectural vision of Downtown Concord. However, they also provided input stating their need for design flexibility, and a desire for variety to provide attractive curb appeal. It was recommended to take a scaled gradient approach, where requirements are more substantial in the core downtown (inner core) around Todos Santos Plaza and reduced as you move away from the core (outer core).

INSERT ADDITIONAL SIGNIFICANT COMMENTS FROM FUTURE MEETINGS

The stakeholders were invited to meetings held on:

September 28, 2015	Housing & Economic Development Committee
October 26, 2015	Housing & Economic Development Committee
November 16, 2015	Housing & Economic Development Committee
February 8, 2016	Todos Santos Committee/Design Review Board
March 21, 2016	City Council
April 7, 2016	Design Review Board
May 19, 2016	Housing & Economic Development Committee
May 23, 2016	Housing & Economic Development Committee
June 1, 2016	Planning Commission
July 12, 2016	City Council

INSERT ADDITIONAL SIGNIFICANT COMMENTS FROM FUTURE MEETINGS

By the early 1600's, the Spanish had claimed and partially settled eastern Florida, southern Texas, the Rio Grande Valley, Arizona (Tucson and Yuma), and southern California. The Spanish towns of the settlements were highly planned on a grid system, based on the Laws of the Indies which provided "ground rules" from Spain for colonial settlements far from home. Many towns and cities in the South and West still clearly reveal the street grid, central plaza, and local Spanish church of the original settlements. Spanish settlements and related architecture can be classified as three distinct types, or functions:

- 1) pueblos, or civic town settlements,
- 2) presidios, or fortified military bastions, and
- 3) missions, or regional churches designed to promote the Catholic faith to Native Americans.

Spanish Mission churches – and simpler local community churches – generally included features derived from the Baroque period in Europe, particularly of the Spanish variety during the 17th and 18th centuries. Baroque features could include rather elaborate facades with round arches, domes, and niches for statuary; prominent belfries or bell towers, curved parapets extending above the entryway, and typically symmetrical facades.

The late 1800s and early 1900s, prior to the Great Depression, saw a prolific

movement in various architectural styles across the country, known as "period styles" or the "Eclectic Era". The Eclectic Era includes both revival and early modern styles that competed ideologically and appeared nearly simultaneously before the Great Depression.

The term "revival styles" was adapted widely across America for use in middle-class homes, wealthy country houses, commercial buildings, early skyscrapers, and civic buildings. Though overlapping with the more picturesque Victorian era, these styles largely gained popularity during the first two decades of the 20th century and heavily influenced residential and commercial landscapes. Most important, revival styles look to the past for inspiration. The trend toward revivalist architecture gained momentum from the 1893 Chicago World's Fair, the Columbian Exposition, where historical interpretations of European styles were encouraged. Simultaneous to the rise of revivalist architecture, the modern era saw its beginnings with architects who were instead looking to the future, not to the past, with more progressive, modernist styles. The Eclectic Movement of the early 20th century, consisted of a simultaneous and competing interest in both modern and historic architectural traditions. This variety, or eclecticism, provided for one of the most diverse and colorful periods for architecture and urban design in American history.

01 | Architectural Overview

One revival style embraced principally in California and Florida, the Spanish Colonial Revival movement enjoyed its greatest popularity between 1915 and 1931. The Panama-California Exposition of 1915 in San Diego, highlighted the work of architect Bertram Goodhue and Carlton Winslow Sr., both who are credited with giving the Spanish Colonial Revival style national exposure. Other revival styles popular at this time along with Spanish Colonial Revival were; Mission Revival, Colonial, Monterey Colonial, Craftsman Style , Prairie Style and French Eclectic. Most of the arts organizations along Balboa Park’s famous El Prado pedestrian walkway are housed in Spanish-Renaissance style buildings constructed for the 1915 Exposition. It was one of the first times that this highly ornamented, flamboyant architectural style had ever been used in the United States.

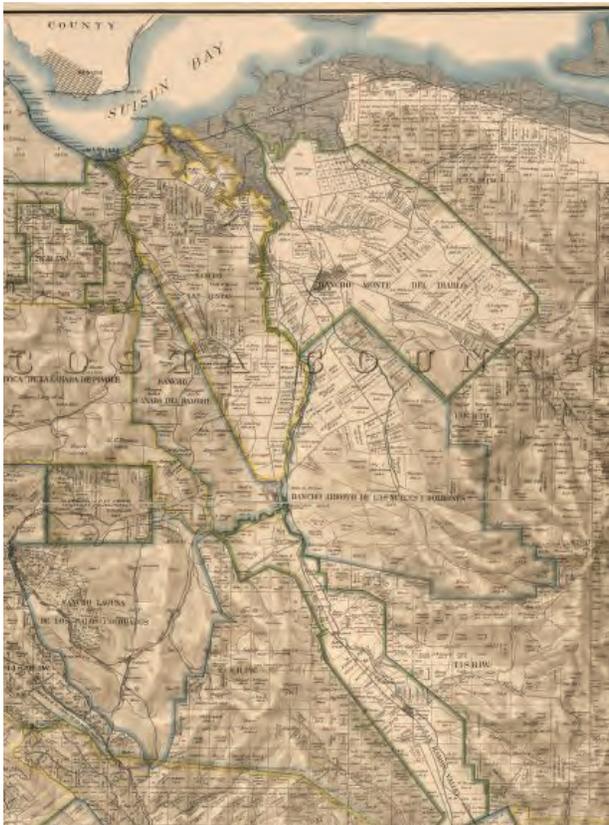
Goodhue and Winslow advocated a design that turned away from the more modest, indigenous, horizontally oriented Pueblo Revival and Mission Revival, towards a more ornate and urban Spanish Baroque. Contrasting with neo-classical Beaux-Arts style, rich Mexican and Spanish Churrigueresque decoration would be used, with influences from the Islamic and Persian styles in Moorish Revival architecture. For American world’s fairs, this was a novelty. The design was intentionally in contrast to most previous Eastern U.S. and European expositions,

which had been done in neoclassical and Beaux-Arts styles, with large formal buildings around large symmetric spaces. Even San Francisco’s simultaneous Panama–Pacific International Exposition held in 1915 was largely in Beaux-Arts style.

After the culmination of the San Diego exposition, Carlton Winslow Sr. stayed and established himself in southern California. In 1925, with the widespread destruction of the earthquake, the City of Santa Barbara persuaded by Winslow, adopted the Spanish style to impart a unified character. Its County Courthouse built in 1925 is a prime example of the Spanish Renaissance style. On March 8, 1960 the City adopted the El Pueblo Viejo Ordinance, a design control district with set restrictions on the alteration or demolition of historic buildings.

References:

Esau, Erika. 1920s: Eucalypts and Spanish Style Architecture, Chapter 6
City of Santa Barbara, Historic Resources Element Adopted 2009
City of Santa Barbara, El Pueblo Viejo Landmark District, Adopted May 12, 2009
Chalquist, Craig. Where did California’s Spanish Architecture Come From?
Mexican California: The Heyday of the Ranchos



Historical Map courtesy Library of Congress, 1894
Alameda and Contra Costa Counties



San Francisco Presidio, 1816



Royal Presidio, Santa Barbara, built 1782



County Courthouse, Santa Barbara, 1925



Larkin Home, Monterey, built 1835

01 | Early California Structures



San Diego Panama-California Exposition quadrangle, 1915



California Tower at Balboa Park, 1915



Casa del Prado at Balboa Park, 1915



Gamble House, Pasadena, Ca, built 1905, Greene & Greene



Colby home, Berkeley, Ca built 1905 by J. Morgan



Shadelands Ranch home, Walnut Creek, Ca built 1902

“The Downtown is distinguished by its authenticity and historic assets, preserved and strengthened by the strategic infill of new high quality development that links the past with a vibrant future. The origins of Concord, beginning in 1834 as Rancho Monte del Diablo, are evident throughout Downtown. The central plaza, which retains the City’s original name of Todos Santos, is a rare example of the 16th Century Law of the Indies which once dictated the planning and design of Spanish colonial cities.

Page 17, Downtown Concord Vision, Downtown Concord Specific Plan

The lands of Contra Costa were assigned in the Mexican land grants of the 1820s and 1830s. During these years, valuable land grants were conferred upon influential men, soldiers and politicians. Soldiers and politicians who had given years of dedicated service to Mexico made up the bulk of those who received these lands. Such people of Spanish descent but born in California, were known as Los Californios. The lands were claimed by a formal petition, called a *diseno*, submitted to the Territorial Deputation Council. A total of 18 grants became the Contra Costa County (*History of Concord, Its Progress and Promise*). The extensive grant of Monte del Diablo included the area that is now Concord. The owner of this land was Don Salvio Pacheco, who petitioned for the grant of four leagues (17,921 acres) in 1834. Salvio Pacheco and his family settled on this vast dominion. He built his adobe near the present center of downtown Concord (*History of Concord, Its Progress and Promise*).

The area of Concord was settled by the founding and settling of Rancho Monte del Diablo by the Pacheco family. Three men are credited for the founding of present day Concord; Salvio Pacheco, his son Fernando, and his son-in-law Francisco Galindo. They hired Alameda County surveyor Luis Castro to survey 20 acres for the new town. Nineteen blocks were plotted around a central plaza.

Don Salvio Pacheco dedicated the plaza to the people of his new town to use as a

park. Salvio Pacheco offered the merchants of Pacheco Town land around the plaza in Todos Santos for one dollar if they would move their stores to Todos Santos.

The reticular or grid pattern is the constant element in the founding of Hispano-American cities where a vision of a uniformed colonial city of the Spanish crown was achieved. The grid pattern of the checker box offered possibilities for controlled expansion, which allowed it easily to become part of the city. The variations of the reticular pattern could be produced by the alignment of the roads or through the arrangement and the dimensions of the blocks.

In 1840s, Salvio Pacheco built the Pacheco Adobe, a two-story home with twelve rooms and a balcony, beside the artesian springs. It was sited to views of Mt. Diablo (*Images of America, Concord*). It is a California State Landmark designated in 1954, and is located at 1870 Adobe Street, at the corner of Salvio and Adobe Streets.

Salvio Pacheco gave his son Fernando Pacheco 1,000 acres on the north side of the rancho. In 1856, Fernando built his own home, the Fernando Pacheco Adobe, on Grant Street (*Images of America, Concord*). The Fernando Pacheco home was entered into the National Register of Historic Places on May 20, 1998 (*Images of*

01 | Architectural Influences in Concord

America, Concord).

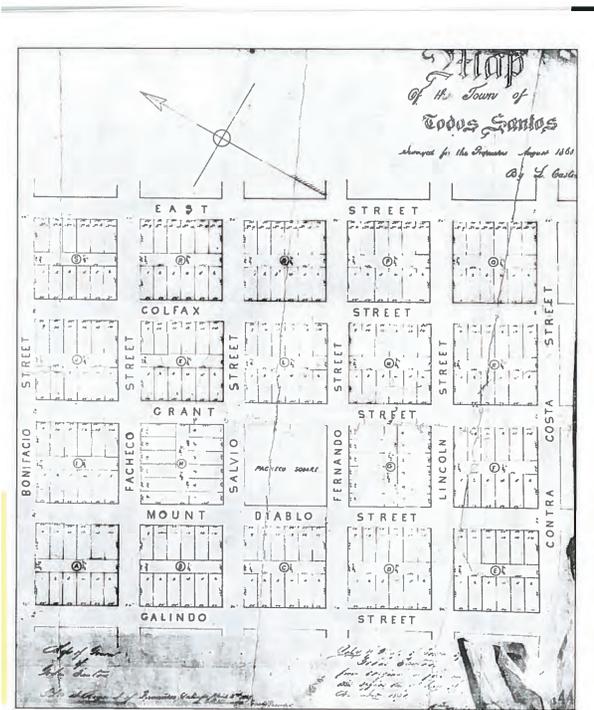
The settlement patterns of Concord closely follow those of California. Early structures in Concord were influenced by architects and craftsman from the East Coast who used their already learned trade and skill to build architecture that was familiar. Early influence was also shaped by an abundance of local materials and popular revival styles brought a vernacular architectural language into new settlement areas. Middle class income residents could choose from one of a dozen or more styles for their home.

Around Todos Santos Plaza many examples of the early revival styles are still evident. Early Concord residential homes and commercial buildings carry the signatures of the Eclectic Era, with a wide variety of architectural styles introduced around Todos Santos Plaza. The Galindo House was built in 1856 at 1721 Amador Avenue near Clayton Road, and serves as an example of a Victorican ranch style home. The Neustaedter House, built in 1906 for the owner of Concord's first pioneer store, is a Tudor style home. The Bibber House on Grant Street was built in 1912-1913 is an example of a four square home with french eclectic details. In 1912-1913 a prairie style home was built for Herbert Elworthy, then Mayor of Concord. The Bolla house was constructed for a local farming family, built in 1924

and is a raised bungalow style home with craftsman details. In keeping with the Law of the Indies, the urban plan encouraged the location of civic buildings around Todos Santos Plaza. The County Fire House built during 1938-39 is a white adobe structure with clay tile roofs, and is still used today by the Contra Costa County Consolidated Fire District. In 1911 Foskett and Elworthy First National Bank was commissioned on the corner of Salvio Street and Concord Boulevard, and is an example of a modern Italianate style building. The references to early Concord buildings can be found on the Concord Historical Society webpage at concordhistorical.org. Images of early Concord residential and commercial structures can be found on the next two pages of these Design Guidelines.

References:

- Harris, Joel A., *Images of America, Concord*. Charleston, South Carolina: Arcadia Publishing, 2009, Print
- Andrews, Edna May, *History of Concord Its Progress and Promise*. Concord, California: Concord Historical Society, 1986, Print
- National Park Service, *National Register of Historic Places in Contra Costa County, California*



The Pachecos hired Alameda County surveyor Luis Castro to survey 20 acres for the new town. Nineteen blocks were plotted around a central plaza. Don Salvio Pacheco dedicated the plaza to the people of his new town to use as a park. Salvio Pacheco offered the merchants of Pacheco Town land around the plaza in Todos Santos for \$1 if they would move their stores to Todos Santos. Some merchants accepted the offer, as well as the Pacheco Odd Fellow's Hall, which was moved to Todos Santos. The first merchant to accept Don Salvio's offer was Samuel S. Bacon, who moved his building from Pacheco to a lot near the new plaza. Bacon built his home next door.

Map of Todos Santos, 1874, by Luis Castro



Salvio Pacheco Adobe, built 1840s



Fernando Pacheco Adobe, built 1856



Bolla Home, built 1924, Bungalow style



Alves Home, built 1906, Colonial style

01 | Early Concord Structures



Galindo Home, built 1856, Victorian style



Neustaedter Home, built 1906, Colonial style



Bibber Home, built 1912-1913, Four Square French Eclectic



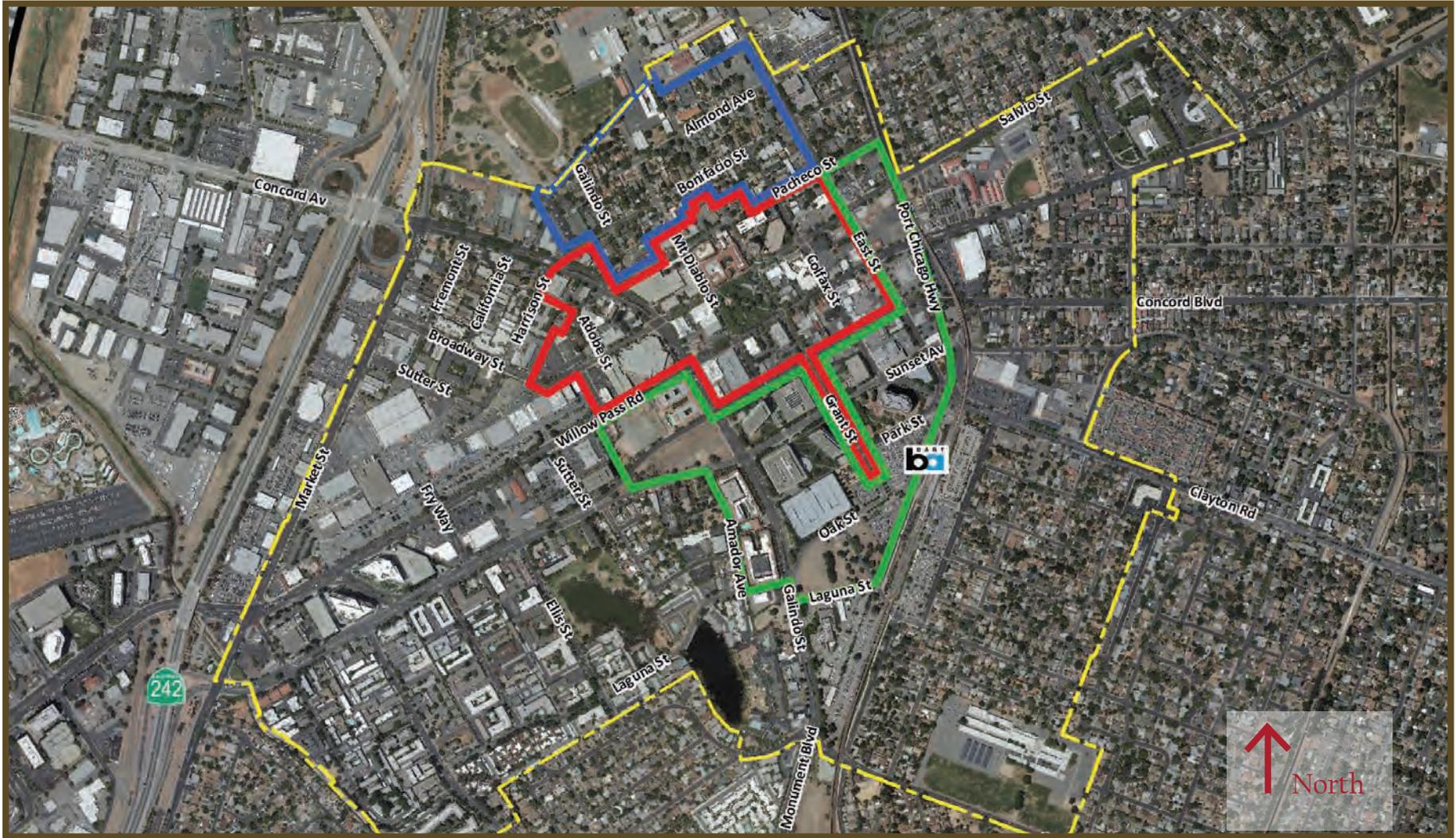
Elworthy home, built 1912-13, Prairie style home



Foskett/Elworthy building, built 1911, Modern Italiane style

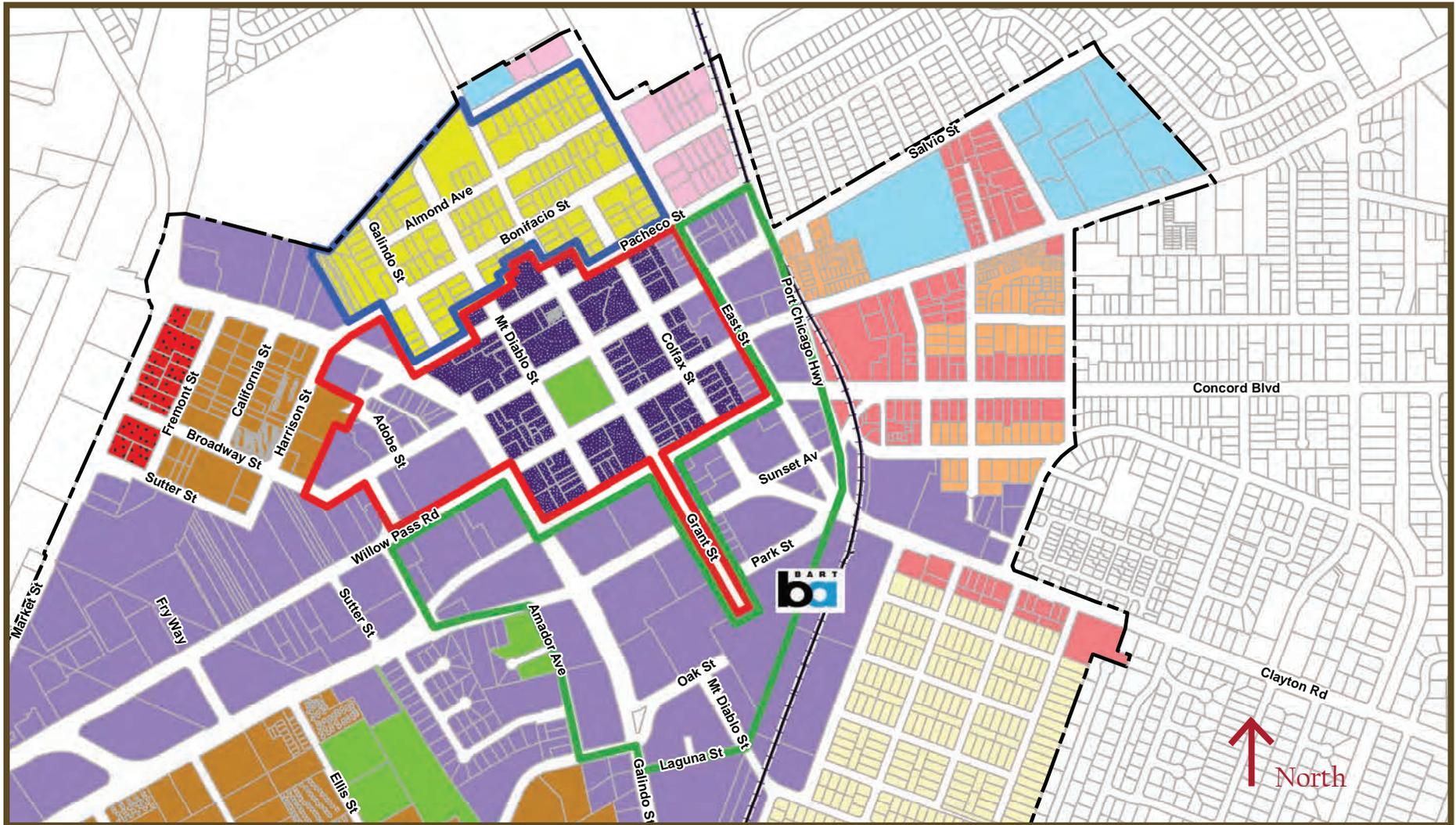


Rosal Apartments, built 1936, Spanish Colonial style



Aerial Map not to scale

-  Priority Development Area
-  City Limit
-  BART Station
-  Inner Core
-  Outer Core
-  North Todos Santos



Enlarged Map, not to scale

Legend		Todos Santos Review Districts		General Plan 2030 Land Use	
	Priority Development Area		Inner Core		Low Density Residential (LDR)
	City Limit		Outer Core		Medium Density Residential (MDR)
	BART Station		North Todos Santos		High Density Residential (HDR)
					North Todos Santos (NTS)
					Community Office (CO)
					Service Commercial (SC)
					Commercial Mixed Use (CMU)
					Downtown Pedestrian (DTPD)
					Downtown Mixed Use (DTMU)
					Public/Quasi-Public (PQP)
					Parks and Recreation (P)



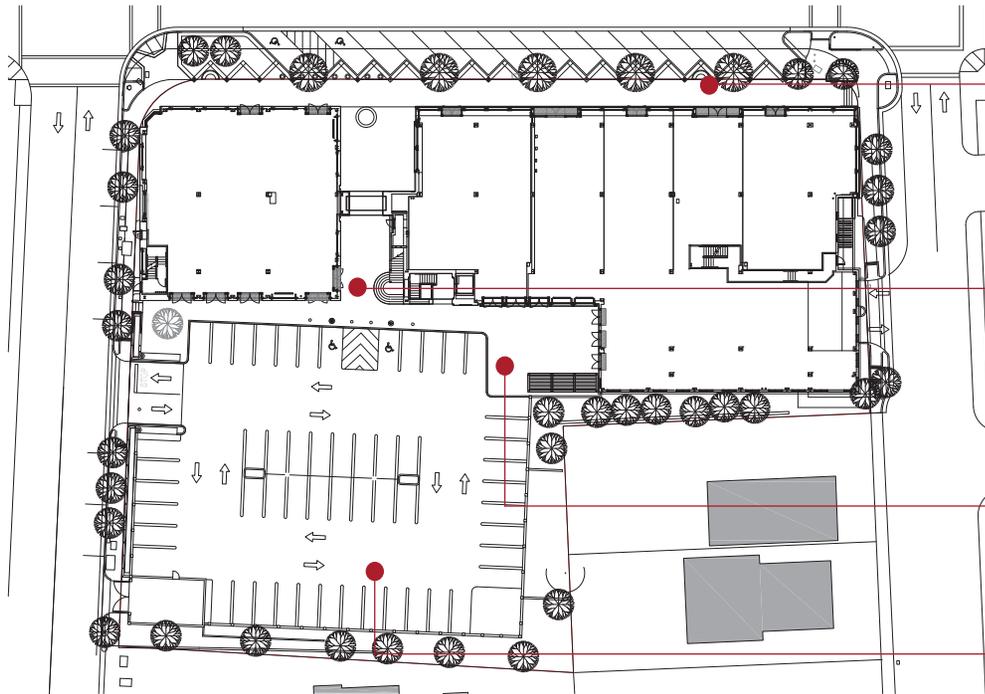
“New buildings shall be designed to be compatible with existing adjacent buildings.”

Downtown Concord Urban Design, March 1987

The goal of these Design Guidelines is to ensure that new development will fit and make a positive contribution to a sense of place. Building form is a basic principle that guides the shape and form of future development. Building form is defined in the Concord Municipal Code by density, floor area ratio, setbacks, lot coverage and height restrictions. In essence it is the building envelope that is allowed on the parcel by the zoning code. In terms of these design guidelines, building form is comprised of various architectural elements including; mass, proportion and scale. In principle, both the Inner Core and Outer Core will adhere to the overall vision for Downtown Concord, the Downtown Specific Plan, the Urban Design Guidelines and through these Architectural Design Guidelines. In the context of Downtown Concord, the overall building form should be informed by adjacent buildings, the character of public spaces such as Todos Santos Plaza, and enhanced pedestrian environment. A neighborhood scale with buildings set close to the sidewalk on a pedestrian friendly street is a desired effect. Development proposals which create a strong building edge with the public right of way are encouraged in the downtown. Facades that create continuity along the street and extend the full length of the street help reinforce a strong building form. A potentially large building form need not be uniform and static. It can be broken into a series of smaller masses that gives variety and visual energy. A range of building shapes, types and styles are encouraged. The inner core building form will be of a more traditional mass and scale; while the outer core building form will have more flexibility, variety and contemporary expression.

02 Building Form

Opportunities to create a sense of place begin with an informed site plan. In a small downtown setting, care should be taken to develop a building site plan that respects the street with a strong activated first floor. Development blocks in the Downtown should have buildings with zero lot lines, parking that is minimally visible and articulation of the overall building form. Buildings with strong design will have a pedestrian designed base, intermediary floors that have rhythm and a top floor that relates to the sky with good roof form. Building mass should be articulated with architectural elements, such as bays, varying the building line, setbacks, and solids versus voids. The scale of the building should be a human scale that relates at the pedestrian level so as to create an overall inviting character and street presence. The proportions of the building should account for adjacent development and be in keeping with the scale of the street. The overall effect of the building form should create a regional architectural identity with design flexibility for the City of Concord.



Site Plan with courtyard style parking

Site Planning & Orientation:

Inner & Outer Core Principles:

- The building has a zero lot line, however it still has movement on the first floor.
- The front building line creates a strong street wall condition to help frame a vital retail corridor.
- The mid-block courtyard space with a fountain helps to relieve a large bulky building, and creates a nice pedestrian space.
- The mid-block courtyard connects the surface parking area and back of house retail to the street.
- Generous sidewalk space and an outdoor eating area enliven the back of house retail spaces and allow street activity to continue to the back spaces.

18.105.040 D 3: *Off street parking shall be located either underground in a parking structure with active storefronts and uses along the street frontages or in surface lots at the rear of buildings that can be accessed from the rear of the property. Off-street parking shall not be located between a building and a pedestrian route to the BART station.*

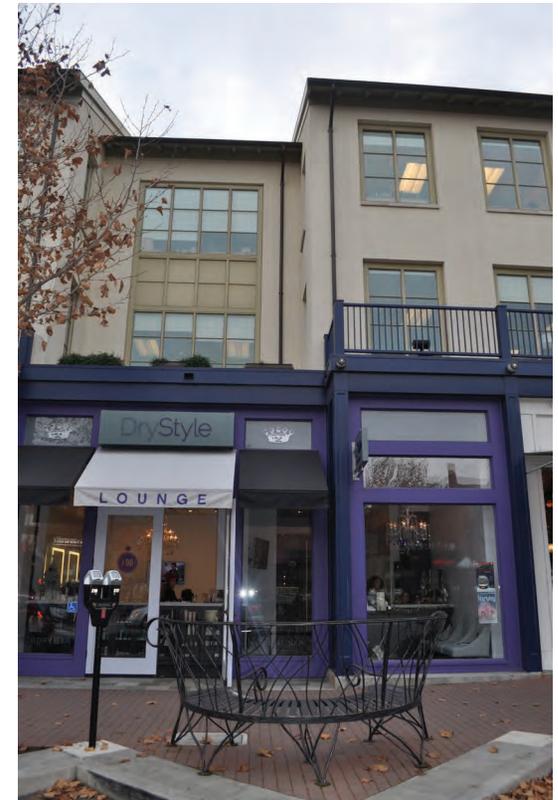
Massing:

Inner & Outer Core Principles:

- Buildings should have a clearly expressed base, middle, and top.
- The upper floors are set back eight to ten feet, creating an inviting and usable outdoor balcony and giving a stronger presence to the first floor.
- The massing of one potentially large building is broken into two smaller masses, acting as two small buildings.
- Setbacks of 4 to 8 feet within a large mass helps to avoid a monolithic appearance.
- The use of at least two distinct materials and or colors creates a dynamic appearance and shape.
- The back of house retail and restaurants are treated like front of house with the same care and detail so they have a main street presence.



Strong building massing



Building Form as it relates to the public sidewalk

02 | Building Form



Scale and proportion in context of an urban downtown

Scale & Proportions:

Inner & Outer Core Principles:

- The scale of the building should be harmonious to the overall context and surrounding environment.
- Desirable relationships between the dimensions and parts of a building help establish the building form.
- Good proportions provide an aesthetic rationale for the dimensions of a building. Architectural design can unify the multiplicity of elements by having all the parts belong to the same family of proportions.
- Good proportions give a sense of order in the facades and spaces of architectural design.
- A more traditional building facade may have classical proportions and an even rhythm for solids and voids.
- A 3 to 4 story building works well on smaller more intimate downtown streets.

One large form broken into two smaller masses



One single mass is divided into three well proportioned elements

Bridging element between two masses

First Floor:

Inner & Outer Core Principles:

- The first floor is taller, allowing for a better connection to the sidewalk. The increased height allows for more visibility.
- The first floor is designed with warm materials such as wood and fabric awnings; materials that lend themselves to being touched.
- Ample fenestration allows passerbys to gaze into shops.
- Retailers take advantage of the window space for unique and artistic window displays.
- Care and detail are given to the first floor with craftsmanship of materials.
- Recessed entrances with double doors help invite pedestrians in.
- The first floor of the building should be articulated with entrances and a pedestrian friendly first floor.
- Human scale helps establish good proportion at the base of a building.



Strong building massing and first floor articulation



Warm quality materials used on the first floor



Active uses engage pedestrians



Strong building entrance and first floor articulation

Urban Design:

Inner & Outer Core Principles:

- The first floor of a mixed use project should have active edges with commercial presence.
- Active edges create a well-defined street-wall to provide continuous retail synergy on the street and sidewalk.
- Mixed use active edge projects should engage a stable amount of pedestrian traffic throughout the day and evening.
- The majority of street frontage should be designed for active uses.
- Design the first floor space with height and depth to support retail spaces, service spaces, educational or cultural spaces.

Successful Strategies:

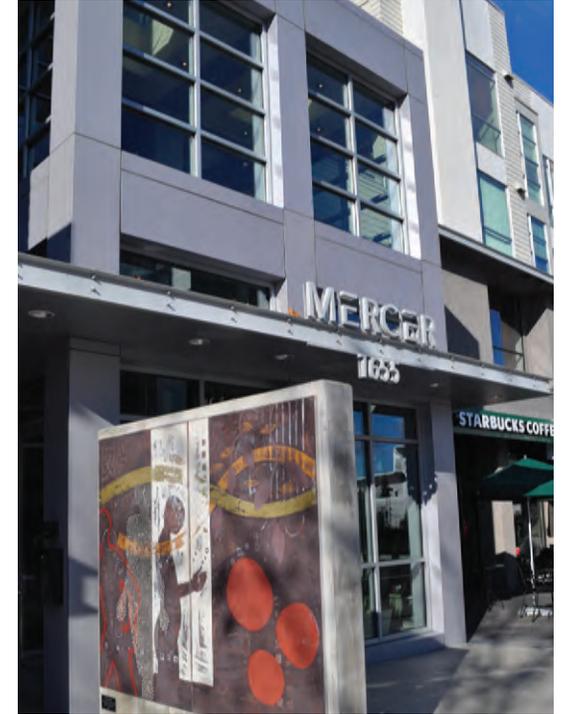
- Design a minimum of 40 foot depth of leasable ground floor depth.
- Plan a height of 18 to 20 feet floor to floor on new commercial development, to allow for a range of use; cafes, restaurants and retail.

18.105.040 C 1: *Continuity of building facades along the street with no interruptions in the progression of building and uses except for pedestrian access.*

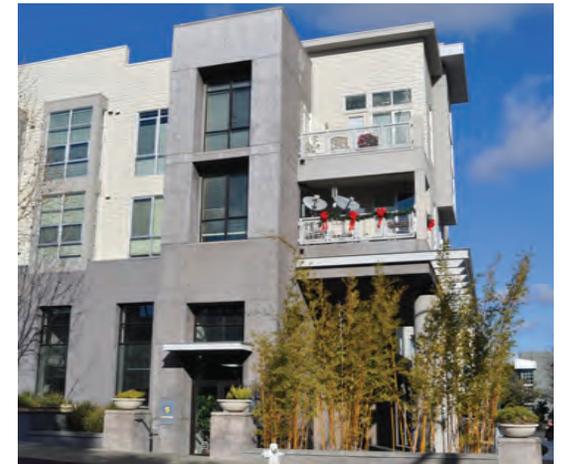
Outer Core Design:

- The outer core follows the same urban design principles as the inner core, with zero lot line, parking below grade, and active retail edges.
- The architectural form maintains a contemporary language through the expression of materials, bays, and volumes. In the contemporary language, elements such as entrances, corners, stairs are more defined.
- The roofline is flat, however there is vertical movement in the bays to create an interesting skyline.
- A contrast in materials, texture and color adds appeal to the facade.
- Details are done in a rational manner so reveals, edges, and grids line up in a deliberate way.
- The main entrance is marked by tasteful artwork.

18.105.040 C 3: *Pedestrian friendly street level building design amenities such as highly articulated facades with a variety of high quality materials and architectural detailing, visibility into buildings, awnings, paseos, or arcades, and signage oriented and scaled to the pedestrian, located directly behind the sidewalk.*



Vertical movement in the bays create interest



Architecture is expressed through material, bays, and volumes



Example of Arts & Crafts style expression



Example of Arts & Crafts materials and details

Inner Core Residential Use:

- Strong massing that hugs the ground gives this three story structure a humble appeal.
- Eaves with deep overhangs shelter the ample balconies.
- Semi-enclosed balconies under a roof help to break up the corner.
- Simple design, sparse detail and natural material add the characteristic features of the Arts & Crafts style.
- Tapered columns give the details for an Arts & Crafts style.
- Window arrangement is grouped in threes.
- Vertical batten board material is contrasted against horizontal lap siding to add variation.
- Combination of low slope roofs and gables add articulation to the roof.

Arts & Crafts Details:

- Columns are crafted with a stone base to define an entrance, and lend a human scale.
- Projecting bays are set within the larger gable form to break up the mass.
- The bay is differentiated from the gable form by varying the siding material.
- Triangular knee brace on the gables gives a distinct quality of Arts & Crafts style.
- Trim is finished in a simple flat wood.

Inner Core Residential Use:

- The overall composition is harmonious and symmetrical.
- Mass that is definitive with projecting and receding bays gives more articulation to the facade.
- Framing around windows and lintels enhances the project details.
- The roofline is varied and helps to articulate the project.
- The base of the building is taller and relates better to the street.
- Roof profile adds a strong statement to the project.
- Window arrangements are symmetrical and aligned.



Italianate Details:

- The facade is finished in a variety of earthtone materials, finishes and colors.
- The project has strong expression at the base with tall arches.
- The eave overhang and depth along with exposed rafters give a traditional expression to the roof.
- Balconies are boxed and framed by wood details.



Example of Italianate style expression



Example of strong base expression

Architectural Element	
Building Form	Heavier mass, formal arrangement of shapes and forms
Building Plan	Symmetrical shape with cross gables and side wings
	Formal plan
Height	Predominantly lower height, one to four stories
Roof Form	Pitched roofs, either low or steep with pronounced parapet or cornice
	Articulated saddles, gables, hip and valleys, dormers and shed roofs
Base of Building	Porticoes, pediments & colonnades, roman semi-circular or squared arches, arcades
	Heavy base rooted to the ground
Upper Floors	Traditional porches and balconies
	Projecting bays
Materials	Warm materials such as stucco, wood, stone, canvas awning, clay tiles
	Glass is a complimentary material
	Earth tone colors
Detail	Terracotta or cast concrete ornamentation
	Cast Iron
	Craftsmanship in details
Fenestration	Wood casement or tall, double hung windows
	Fenestration set within the mass
Amenities	Interior or exterior courtyards, balconies



Spanish Colonial Style Mixed Use



Colonial Style Retail

02 | Inner Core Design Elements



Mission Style Multi-family Residential



Arts & Crafts Style Residential



Italianate Style Residential



Arts & Crafts Style Retail



Monterey Colonial Style Retail



Colonial Style Mixed Use Retail

Architectural Element	
Building Form	Geometric forms, planar elements and asymmetrical volumes
Building Plan	Functional elements such as stairs, entrances, chimneys have individual expression
	Open floor plan with exposed structural elements
Height	Four to eight stories
Roof Form	Predominantly flat, sloped lines or angular with clean parapet
	Multiple roof lines at various levels with squared edges
Base of Building	Planes and walls define entrances
	Columns and suspended overhangs
Upper Floors	Porches and balconies that are cantilevered or suspended
	Projecting or recessed bays
Materials	Masonry, glass, metal, concrete, tensile structures, large spans of fenestration
	Roof may be metal
	Earth tone colors
Detail	Reveals, edges and grids line up in a rational way
	Expressed through variation and contrast in material
Fenestration	Large spans of window walls act as a feature
	Floor to ceiling, frameless or minimal framing
Amenities	Interior or exterior courtyards
	Rooftop gardens



Residential development near BART



Restaurant and retail development

02 | Outer Core Design Elements



Residential development near BART



Mixed use development with residential near BART



Mixed use development with residential near BART



Mixed use with restaurant and retail uses



Two story retail and office



“A city is an ecological balance of things Public and things Private.
Fenestration is the portal between the two.”

Geoff Ferrell, founder
Form Based Institute

The openings between a building and the street, especially windows and doors, establish a relationship that helps to characterize our sidewalks and streets. The arrangement, shape, height, type and amount of fenestration dictate how we interact in a downtown. Fenestration plays a key role in how a building can create an inviting public space and add liveliness to a downtown area. In the context of these Design Guidelines, fenestration can improve the compatibility of the building to the sidewalk and the overall public realm. The windows on the first floor of a building will often be the first invitation into the building. This is important especially for a mixed use building with retail on the first floor. The windows and doors are architectural elements to draw a pedestrian into an inviting retail space. The interaction of windows on the first floor with the sidewalk creates a dialogue, energizing and making a sidewalk space more interactive. A more engaged first floor and sidewalk area make for more walk-ability. The window arrangement and its rhythm and pattern allow us to understand if the space behind is public, such as retail, semi-public, such as offices, or private, such as residential. In a traditional building upper floors may have smaller windows, to create a more semi-private realm while a more modern building may have larger openings and be more transparent. Regardless of the size of the openings, fenestration is an important architectural element and lends character to the facade and ultimately the downtown context.

03 | Fenestration

In a well designed neighborhood, the ground floor of a building works in concert with the surrounding sidewalk and public spaces. An active first floor with continuous retail synergy lends to an engaging public realm. Having windows and welcoming front doors of a building face the public street can create a sense of security and comfort for pedestrians. A public street edge should have a well-defined streetwall to help frame public streets and public spaces, such as parks and plazas. Locating the facade at the property line, or zero lot line condition helps to create an urban edge. The urban edge should have strong connections with adjacent buildings, public sidewalk/plaza. Major entrances should be located on public streets and or corners. Transparent materials should be used to invite pedestrians into storefronts, along with interior lighting to maximize visibility of street level uses. Strategies such as transparent glazing, large welcoming openings, windows, and outdoor seating help to reinforce a strong connection. Service areas, loading areas and parking entrances should be located off the public street and preferably behind the building where it will be screened.



After image of renovations

Successful Strategies:

- Varying the facade material, parapet, colors and patterns creates a economical yet effective solution.
- Doors are recessed and the transition areas are angled to welcome passerbys in.
- The window glazing has been maximized to open towards the street and sidewalk.
- The arrangement of the fenestration gives the building rhythm.
- Design a continuous band of retail and restaurant spaces help to energize the street level. Offices and service uses are kept off the main streets.
- Create a ground floor flush with and accessible from the sidewalk.

Re-model & Renovation:

- Although the existing building is just over one acre, and comprised of several parcels, it was strategically broken into a small series of retail shops to give a small downtown appeal.
- On the re-model of Peet's Coffee, care was taken to keep details such as the dentil crown molding on the parapet and add new copper gooseneck lighting.
- A series of windows spaced between the new pilasters, helps the Peet's Coffee store sets up a vertical rhythm along the sidewalk.
- A unique presence is created by having painted art panels aligned above each storefront window. The awnings act as a horizontal band that ties the art panels with the storefront windows.
- Each retail store ties in with neighbors, yet still has an individual branding and look. There is a richness in the variety of facades.
- The existing rooftop cupola was modified to give better vertical proportions and add corner presence.

18.105.040 E: *Exterior building walls facing any street frontage shall include windows, doors, or other openings for at least 50 percent of the building wall area located between two and one-half and seven feet above the level of the sidewalk. No wall shall run in a continuous plane for more than 20 feet without an opening. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces.*



Before image of renovations



Generous amount of fenestration and recessed doors make for an inviting retail space.



Color and change of material engage this retail space with the sidewalk.

Inner Core Retail:

- Interest is created in the storefronts by varying the pattern of tile.
- Fenestration is encased by a two foot base at grade.
- The window glazing is framed on all four sides so that it is contained and acts to enclose the storefront.
- Human scale elements are introduced at the top of the storefronts by solid fabric awnings. The awnings also serve to provide shelter from inclement weather.
- Entrances are set back from the building line.
- The building height is kept consistent, and interest is added through varying materials, different color awnings and handcrafted details.

“And third, the sidewalk must have users on it fairly continuously, both to add to the number of effective eyes on the street and to induce the people in buildings along the street to watch the sidewalks in sufficient numbers.”

*Jane Jacobs, author
The Death and Life of Great American Cities*

Outer Core Retail:

- Vary the facade height to lend to a strong corner presence.
- Extend the fenestration from the grade to ceiling to allow the interior ceiling to be exposed.
- Extend window glazing two stories to frame a two story storefront for a contemporary appearance.
- Human scale elements are introduced at the top of the storefronts by horizontal grid steel canopies that cantilever out.
- The corner is more pronounced through the tall two story height columns and angular roof.
- The storefront glazing meets the ground so that inside and outside are seamlessly connected.



Large spans of fenestration help enliven a two story space.



Retail storefront extends to the grade.



01

El Pueblo Viejo Landmark District Santa Barbara, California
East Ojai Avenue Design Guidelines, City of Ojai, January 2001
Balboas Park Central Mesa Precise Plan, San Diego, California
National Park Service A National Register of Historic Places Contra Costa County

02

Downtown Corridors Plan Design Guideline, Adopted xx, 2016
Downtown Concord Specific Plan, Adopted June 6, 2014
Downtown Concord Urban Design, Concord, Ca, March 1987
City of Concord 2012 Development Code, Adopted July 24, 2012
North Todos Santos District Design Guidelines,



EARLY CALIFORNIA AN OVERVIEW

HOUSING & ECONOMIC DEVELOPMENT SUBCOMMITTEE

Community & Economic Development

Afshan Hamid & Joan Ryan: September 28, 2015

Objectives:

One of the six Land Use/Economic Vitality Objectives in the Downtown Plan is to:

Reflect early California architecture in the design of new buildings.

Major Policies

- ☐ New development to provide consistency in character that enhances and reinforces the existing character of the Downtown.

Implementation Strategies

- Design Guidelines to provide clarity for new development in terms of building massing, exterior articulation, and material palette for consistency with Downtown.
- Optimize Design Review Process to reinforce Design Guidelines and Objectives.



Objectives:

*Provide a variety of living opportunities through a range of housing types and prices.
Promote a diverse mix of ages, incomes, and cultures among residents and employees.*

Major Policies

- Encourage a diversity of housing types for all different types of users.
- Promote higher density housing near transit.
- Maintain and create more affordable housing in the Downtown.

Implementation Strategies

- Engage a wider range of developers for Successor Agency-owned sites to encourage more diversity in housing development.
- Continue to collect in-lieu fees to invest in/rehabilitate affordable housing.
- Provide density bonuses for amenities (day care, etc.) provided by developers.





Early California settlement:

Spanish and Mexican occupation

- Law of the Indies

- Settlement pattern for new towns

- Codified city planning process and first general plan



Presidios (fortified military bastions)

Missions (regional churches)

- Colonize pacific coast region

- Spread Christianity



Pueblos (civilian towns)

- Sufficient space for recreation and cattle

- Elevated, healthy location with fertile soil

- Have fresh water

- Centered on Plaza Mayor (main square)

- Rectilinear grid and 12 straight streets



Other influences:

East coast architects

Victorian

Abundance of adobe material

Construction of 21 missions in adobe material

Secular construction of presidios, pueblos & ranchos

Eclectic Era:

Romanticize the past

Age of Eclecticism (includes modern and revival styles)

Overlap with Victorian era

Revival Styles:

Monterey Colonial

Mission Revival

French Eclectic

Colonial Styles (Spanish and New England Colonial)





Panama-California Exposition of 1915, San Diego, Ca

Spanish Colonial Revival movement national exposure

Popular between 1915 – 1931

Contrast to modest pueblo and mission style of architecture

Ornate and urban Spanish Baroque style

Rich Mexican and Spanish Churriguerisque decoration

Influences from Moorish architecture



California Building a stylistic interpretation:

Highlight of work of architect Bertram Goodhue & Carlton Winslow Sr.

Stylistic and period references

Re-capitulated history of Spanish Colonial in North America

Mexican Baroque

Vernacular styles adopted by Franciscan mission on California coast



Spanish Colonial Revival:

Popularity of style led to the name Spanish Colonial Revival

First time flamboyant style used in the U.S.

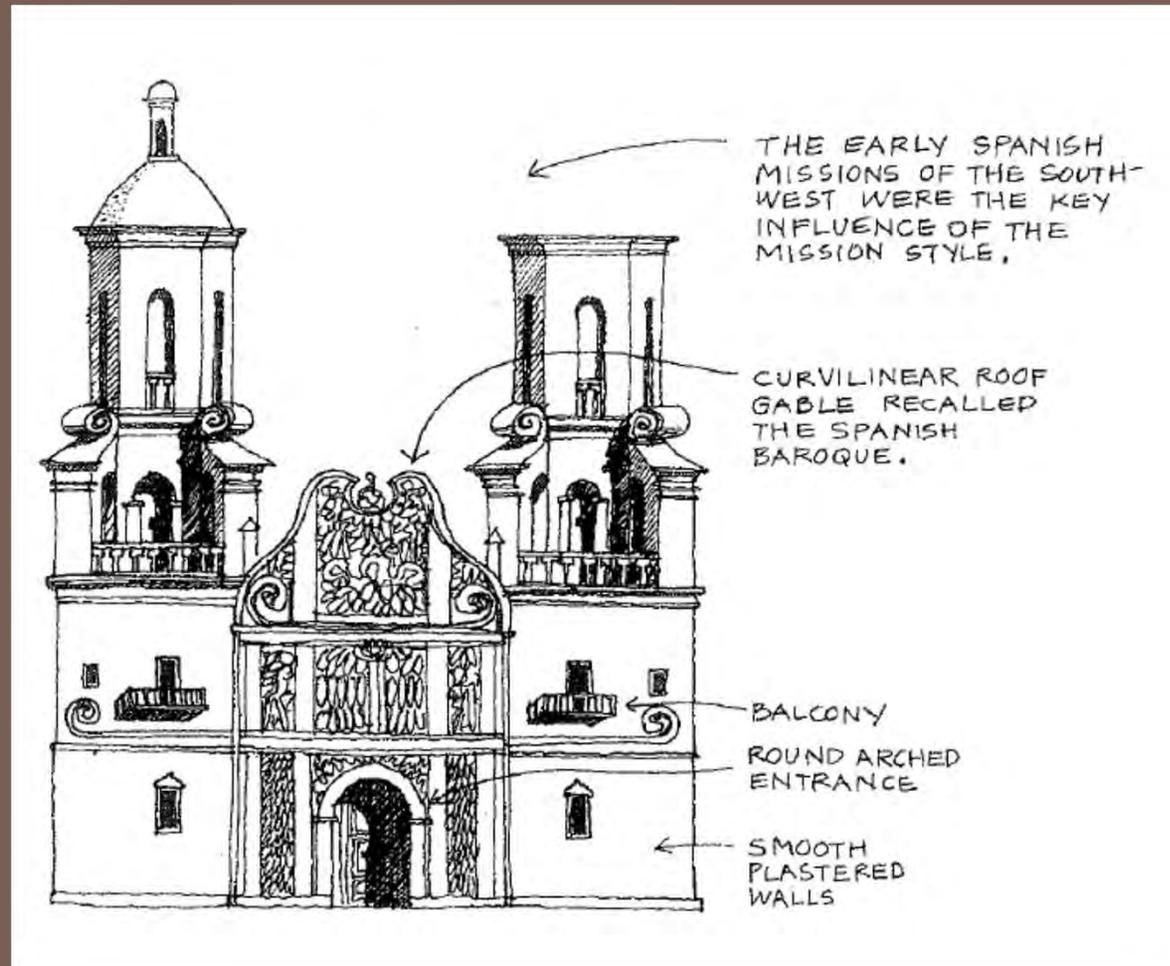
Became a historical vernacular style

Winslow stayed in southern California

Instrumental in persuading the City of Santa Barbara to adopt Spanish Colonial Revival after mandated civic style after 1925 earthquake

Common Features:

elaborate low relief carvings
curvilinear roof gables
balconies
stucco or plaster walls
arcades
towers
decorated parapets
elements of Baroque





Barbara:

Destructive 1925 earthquake credited for unified architectural theme.

Court house built in 1929, significant civic building.

Spanish and moorish design elements led to Mission Revival

Large arch with keystone, and decorative medallions

Tower element

Adobe material, white washed finish, recessed doors and entries



El Pueblo Viejo district:
Preserve and enhance heritage and
architectural character of the central
city.

Heart is the Royal Presidio, b. 1782

Laws governing architectural style
within the district.



Compatible with Hispanic Architecture
18th c. and 19th c. styles, including:

California Adobe

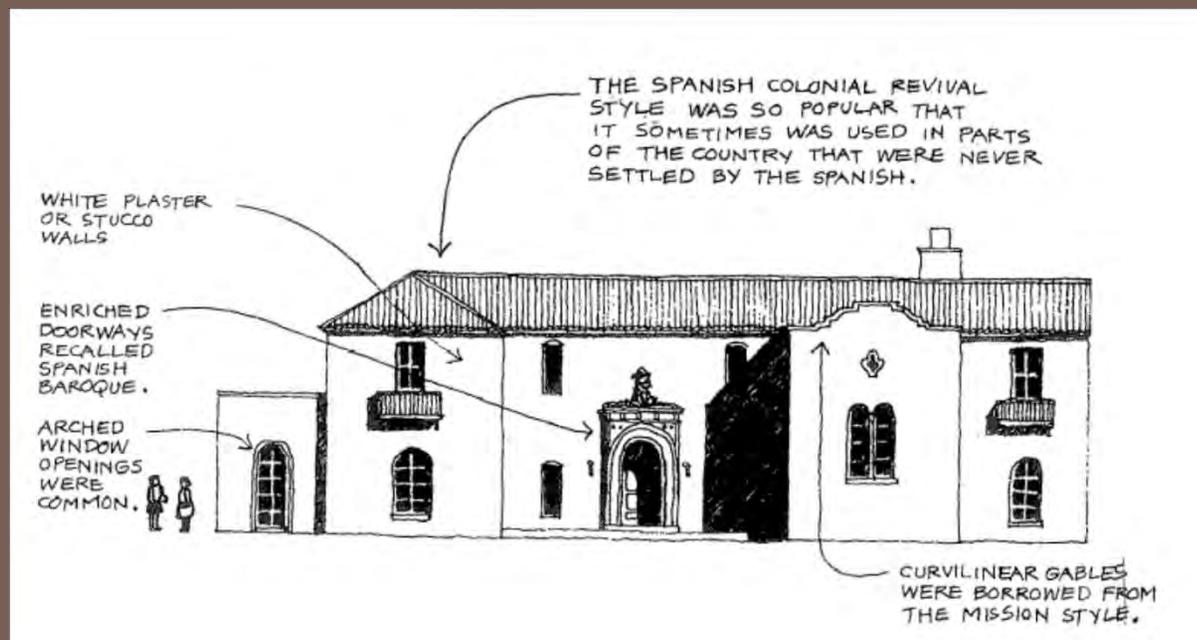
Monterey Revival

Spanish Colonial Revival

Revival styles 1915-1930

Common Features:

- stucco surfaces
- red tile roof
- arched openings
- balconies
- logias
- decorative ironwork





Monterey Colonial:

New style of architecture evolved with unification of Spanish-Mexican and New England practices.

Larkin house

Timber framing with adobe walls,

Second story

2 rooms on either side of a central hall

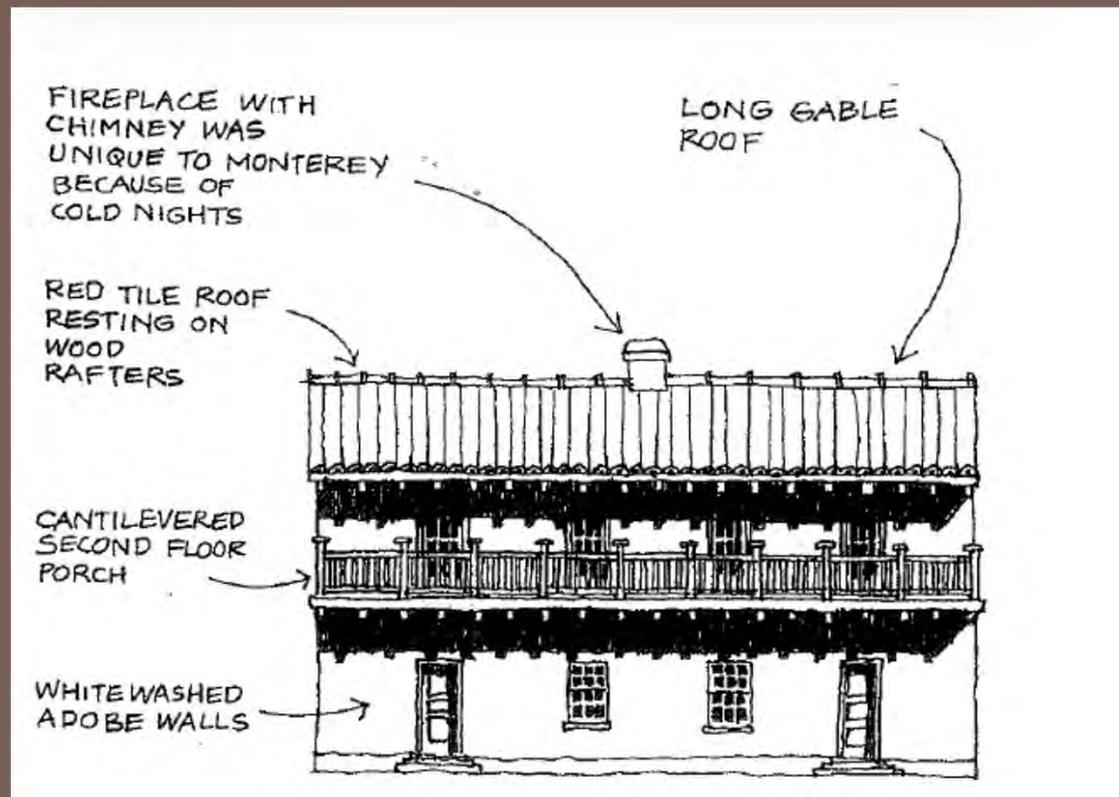
Two story veranda

Native redwood material



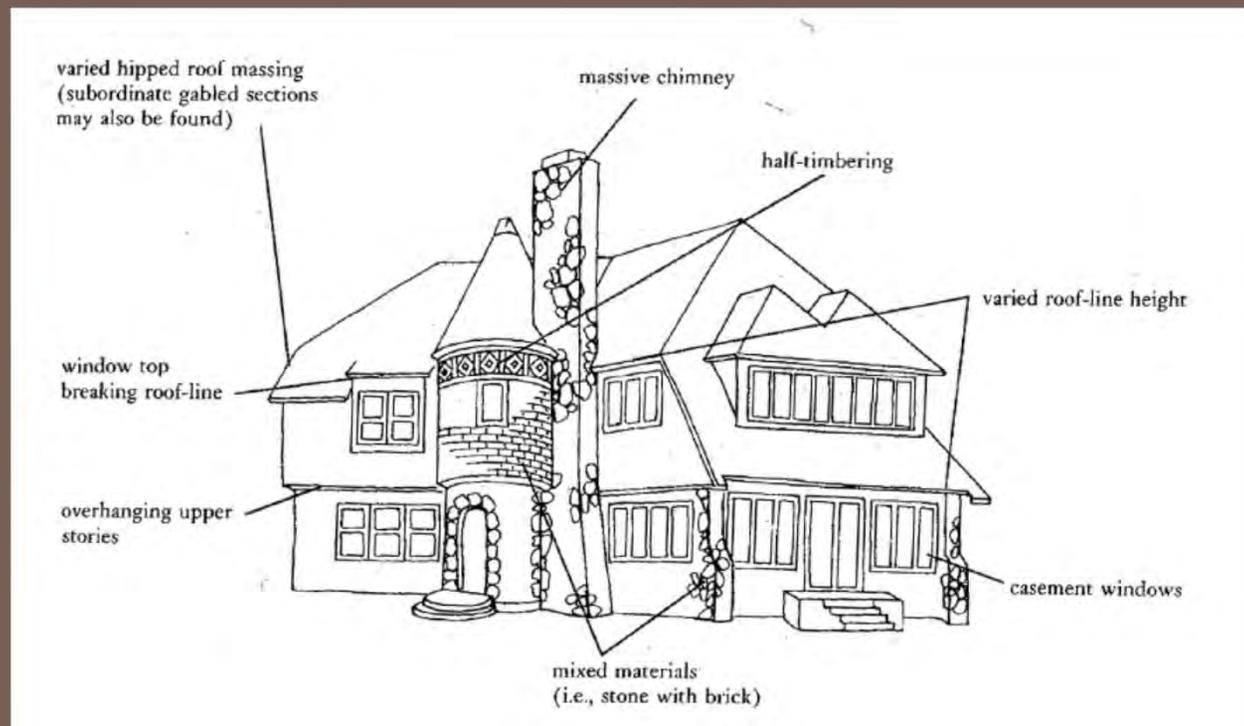
Common Features:

- Wood shake or mission
- Red tile roof
- Second story porch
- Arched windows
- Stucco or plaster walls
- Window shutters
- Asymmetrical layout



Common Features:

- steep gable roof
- stucco exterior
- small porches
- large central chimney
- towers and silo towers

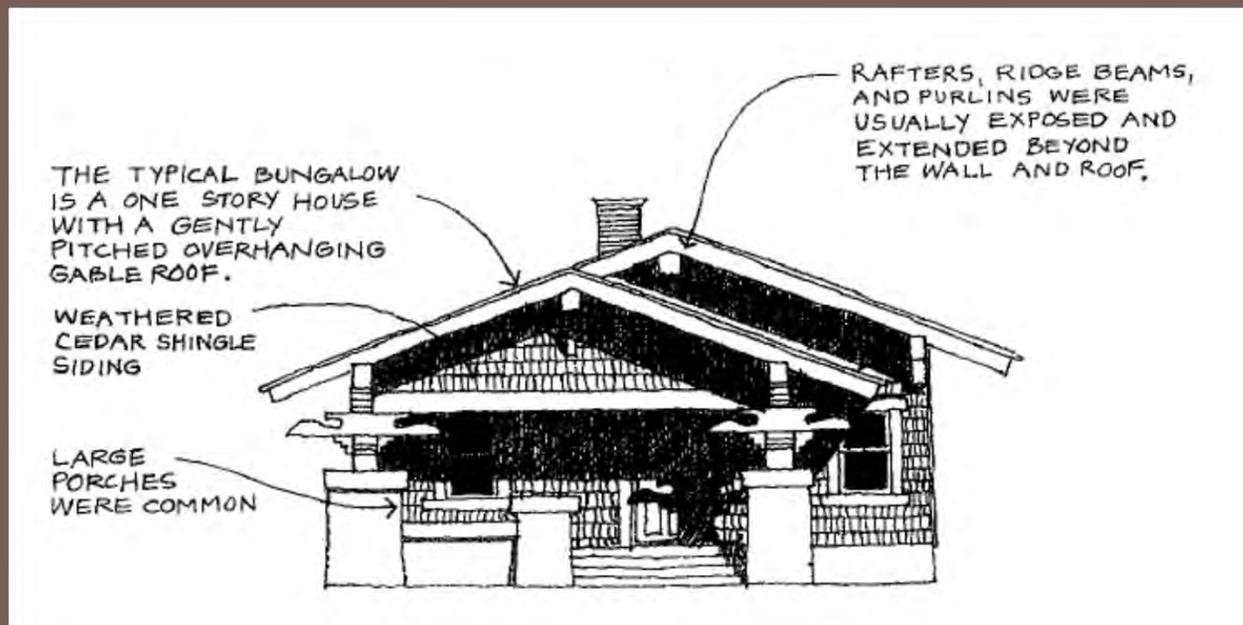


Common Features:

- open floor plan
- large front porch
- front stoop
- raised first floor
- low pitch roof
- integrated natural surroundings

Influences:

- Mediterranean
- Vernacular
- California



Don Salvio Pacheco house

Adobe Street:

Adobe walls

Wooden beams to support second story

Views to Mt. Diablo

Built late 1840s

Father of Concord



Fernando Pacheco house:

One story rancho style

Adobe structure

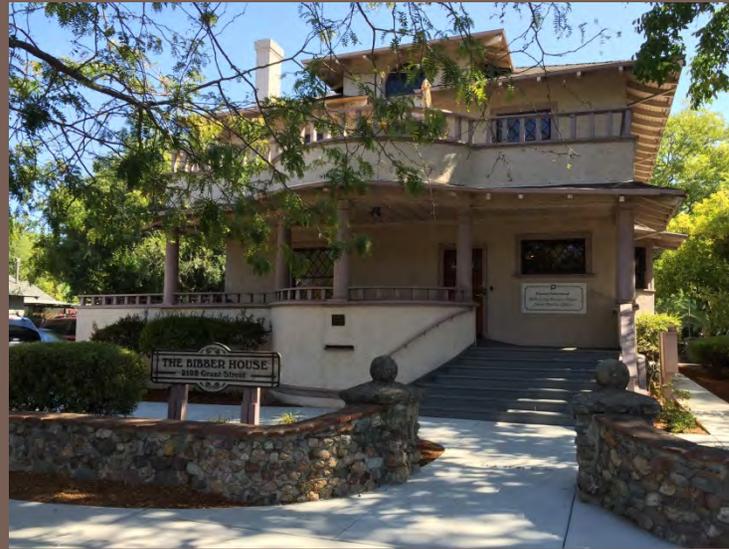
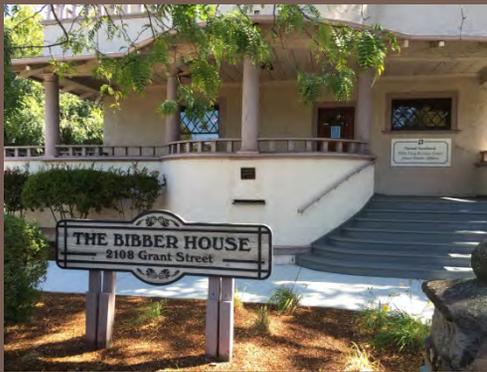
Built 1850s

First settlers of Concord



Bieber House, Grant St:

eclectic &
low



taedter House, nt St:

low style
atch roof
ous front porch



La house, Bonifacio St:
Bungalow style



Ives House, Grant St:
Colonial Revival style
Central plan
Pediment over entry



Galindo house, Amador Ave.:

- Victorian style
- Projecting 2 story bay
- Shake roof
- Horizontal wood siding
- Raised porch
- Vertical design



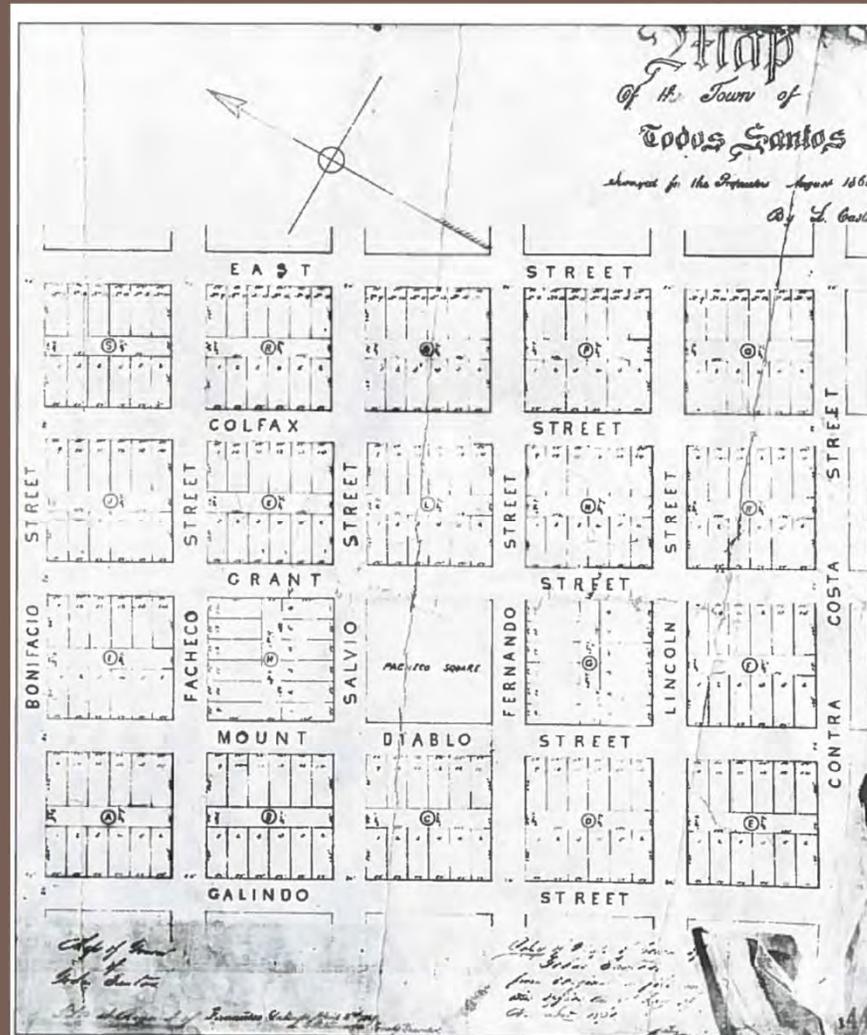
Plans of Contra Costa:

Spanish Land Grants 1820-1830s.

Grant of Monte del
included Concord.

Castro surveyed 20 acres
new town. 19 blocks
around a central plaza.

followed the Law of the
traditional urban
concept.









DISCUSSION & QUESTIONS



:
. Civic Dr.
mit market rate apt
l parking structure
ies with pool and
area
for one bedroom



BRIO APARTMENTS



MacArthur Station:

Mixed-use transit oriented
development

Adjacent to BART station in
North Oakland

624 housing units,
108 units BMR



Berkeley Plaza:

211 Harold Way

Historic context and contemporary
character

102 new units

18 story building

Access to public transit

Landscaped roof top terraces

Photovoltaic trellis

Corner element

Setbacks at the 4th level





Domain Apartments:

1389 Jefferson St.

264 apartment unit

Transit Oriented

Walkable

Gym and yoga studio

Bike workshop



**REPORT TO COUNCIL COMMITTEE ON
HOUSING AND ECONOMIC DEVELOPMENT****TO HONORABLE COMMITTEE MEMBERS:**

DATE: September 28, 2015

SUBJECT: DISCUSSION OF EARLY CALIFORNIA ARCHITECTURE FOR THE DOWNTOWN**Report in Brief**

The Downtown Concord Specific Plan (Specific Plan) includes a chapter on Design Guidelines to provide guidance to property owners, developers, the City's Design Review Board and staff regarding the City's expectations for the architectural design of new development or exterior building remodel projects in the Specific Plan area. In the Specific Plan, a short-term implementation measure (Measure D-1A) directs that a study session will be conducted to discuss and explore the Early California Architecture theme with the intent of determining whether the City is interested in developing more defined Architectural Design Standards with the Early California theme for development within the downtown. This Study Session begins that discussion.

Background

The City Council unanimously adopted the Specific Plan on June 25, 2014, which includes a chapter on Design Guidelines that focuses on urban form, massing and character, ground floor treatment, relating facades to the public realm, vehicular access and building design including parking, servicing and private open space. The discussion of building character was general in nature and not overly prescriptive, encouraging high quality architecture, and other broader recommendations. The guidelines noted that a number of historic buildings in the project area exemplify Early California architecture. The guidelines in the Specific Plan identified desirable design elements that reflected the character of these historic buildings:

- Breaking up single large block buildings into a smaller series of buildings/variation in the facades to create a finer building grain fabric;
- Providing important roofline articulations/stepping back the top floors of buildings;
- Ensuring that the ground floor of a building relates to and enhances the public realm/streetscape;
- Providing deep reveals for window treatments;
- Incorporating balconies with permeable railings;
- Using a common materials palette; and
- Utilizing arcades along specific streets.

The focus of the guidelines (included as Attachment 1) was to provide guidance to property owners, developers, the City's Design Review Board and staff for evaluation of the architectural design of proposed projects in the Specific Plan area. The guidelines describe the desired urban form and massing, setbacks and ground floor treatment rather than a specific architectural style or theme to be used for new development and remodel projects. The scope, budget and timeline of the Specific Plan, did not allow for preparation of architectural Design Standards beyond these Design Guidelines. Instead, a short-term (2014-2017) Implementation measure (D-1A) was included to further explore the Early California architectural theme. Some

members of the City Council have voiced a desire to see stronger tools in place to ensure that the architecture of much new development in the downtown area aligns with an Early California architectural style. The Specific Plan boundary within the downtown is shown in Attachment 2.

The City's General Plan does refer to the North Todos Santos (NTS) Specific Plan, adopted by the City in 1985, noting that it is intended to protect the character of this historic neighborhood. The North Todos Santos area is generally bounded by Concord Avenue, Pacheco Street, Port Chicago Highway and Mt. Diablo High School on the north, and coincides with the North Todos Santos zoning district which is within the larger overall boundary for the Downtown Specific Plan. The Design Guidelines that evolved from the NTS Specific Plan are attached as Attachment 3. Additionally, the City has attached the Downtown Concord Urban Design book, adopted in March 1987, which presents policies for urban design, for use by developers, sponsors, and individuals "who have an interest in the quality of Concord's downtown environment" (Attachment 4).

Discussion

The term Early California architecture can be viewed as a mixture of architectural influences that have combined over time, resulting in a variety and melding of architectural styles. The attached photo examples summarize the various architectural styles used in the early periods of California history for reference (Attachment 5) and discussion.

Spanish Revival Architecture

Spanish Revival architecture emerged as a result of the Spanish mission sanctuaries that were established throughout California in the late 18th and early 19th century, and was a revivalist architectural movement that began in the late 19th century, drawing inspiration from the earlier missions. Common elements include arched corridors, long arcades and generous courtyards typically with a fountain, wide projecting eaves, and low sloping tile roofs. Materials including massive adobe walls, timber, stone, brick and tile typically represent the style. Terraced bell towers are often incorporated. Today, stucco walls, with arched door and window openings and tile roofs have become the standard Spanish Revival appearance and Salvio Pacheco Square, on the north side of Todos Santos Plaza, incorporates many elements of this style. The City of Santa Barbara is well known for its Spanish Revival architecture and has many excellent buildings that represent this style. The El Pueblo Viejo Design Guidelines for City of Santa Barbara have been attached for reference (Attachment 6).

Monterey Colonial Architecture

The Monterey Colonial style verandas more than likely harkened back to the Spanish as well. This architectural style typically included adobe buildings, with low-pitched gable roofs, and a balcony overhanging the entrance door, cantilevered and supported by wood beams or posts. Most examples of the style use a mixture of wall materials. Stucco over the brick veneer can provide a rustic effect. The Don Salvio Pacheco Adobe at 1870 Adobe Street, known to be the first structure in Concord, is an example of this architecture.

Victorian Architecture

The Victorian architectural style evolved in the mid to late 19th century and refers to the reign of Queen Victoria (1837-1901), however many of the recognized elements did not become popular until the late 1800s. A

variety of well-known styles emerged from the era including Queen Anne, Italianate and Gothic Revival styles. As the movement moved west to California, Victorian architecture generally described styles popular between 1860 and 1900 and the Queen Anne style came into fashion in the 1880s. These elaborate, brightly colored homes are the image most think of as a Victorian home. San Francisco is well known for its Victorian buildings, including the iconic “painted ladies”. Typical elements include two to three story large buildings with a one-story porch, wood or stone exterior, bay windows, ornamental brackets, cupolas, and a high degree of decorative trim detailing in vibrant colors. The historic Galindo House represents the style within Concord.

Mission Revival

The Mission Revival style (1910-1940) originated in southern California and is similar to its “relative,” Spanish Revival. Common elements include a distinctive curved parapet, roof overhangs with visible rafters or roof brackets underneath, square towers, stucco cladding, red tile roofs, arched porticos, round arches and ornamental iron accents.

Local Historical Examples

The Concord Historical Society maintains a map and tour on their website of 28 historic places in downtown Concord. <http://concordhistory.com/tour/> The City also maintains a map and listing of those sites and structures, which is attached as Attachment 7.

Private Development

There may be certain challenges faced by developers with incorporating Early California architectural design into higher density office or multi-family projects and therefore staff has invited developers that have shown an interest in development within the City to attend this meeting to share any input they may have with respect to this subject.

Next Steps

Staff seeks input and guidance from the Committee as to better defining this issue and requests direction on the following points:

- 1) Confirmation of architectural styles and elements that define Early California Architecture and whether there is a desire for preparation of a subsequent more specific architectural design guide, or design standards, and if so, begin to define the scope.
- 2) Define a boundary, either the entire Specific Plan area, or a subarea within the Specific Plan, that would be subject to new specific Development Guidelines or Standards if that is the chosen direction.
- 3) Types of development, i.e. thresholds you would consider, for projects that would be subject to any new architectural style requirements and whether those include more defined Design Guidelines (recommendations) or Architectural Design Standards (requirements). For example, types of development could include all new projects, or just commercial projects, projects of a certain

square footage and/or residential projects of a certain size, and setting similar thresholds for renovation or remodeling projects.

Public Contact

The agenda item was posted. Developers with an interest in the downtown have been notified.



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- Attachment 1 – Downtown Concord Specific Plan, Chapter 4: Design Guidelines, June 2014
- Attachment 2 – Downtown Concord Specific Plan Boundary
- Attachment 3 – Design Guidelines for the North Todos Santos Neighborhood, 1985
- Attachment 4 – Downtown Concord Urban Guidelines, dated March 1987
- Attachment 5 – Early California Architecture briefing
- Attachment 6 – City of Santa Barbara - El Pueblo Viejo Design Guidelines
- Attachment 7 – Concord Historical Sites and Structures map



DOWNTOWN CONCORD SPECIFIC PLAN

Design Guidelines

Adopted June 25, 2014



04 Design Guidelines

4.1 GENERAL INTENT

The general intent of this section is to provide a set of building development design guidelines to provide guidance to potential property owners, developers, and the City's design review/staff for determining the architectural character and building development for proposed projects. As the current City of Concord's Development Code has specific standards for height and FAR, as well as setback regulations, this chapter focuses on building character, how buildings should relate to the public realm, and address parking and servicing to ensure the strong pedestrian character of the Downtown and area around Todos Santos Plaza is maintained.



Salvio Street, 1930's



Todos Santos Plaza, 1915

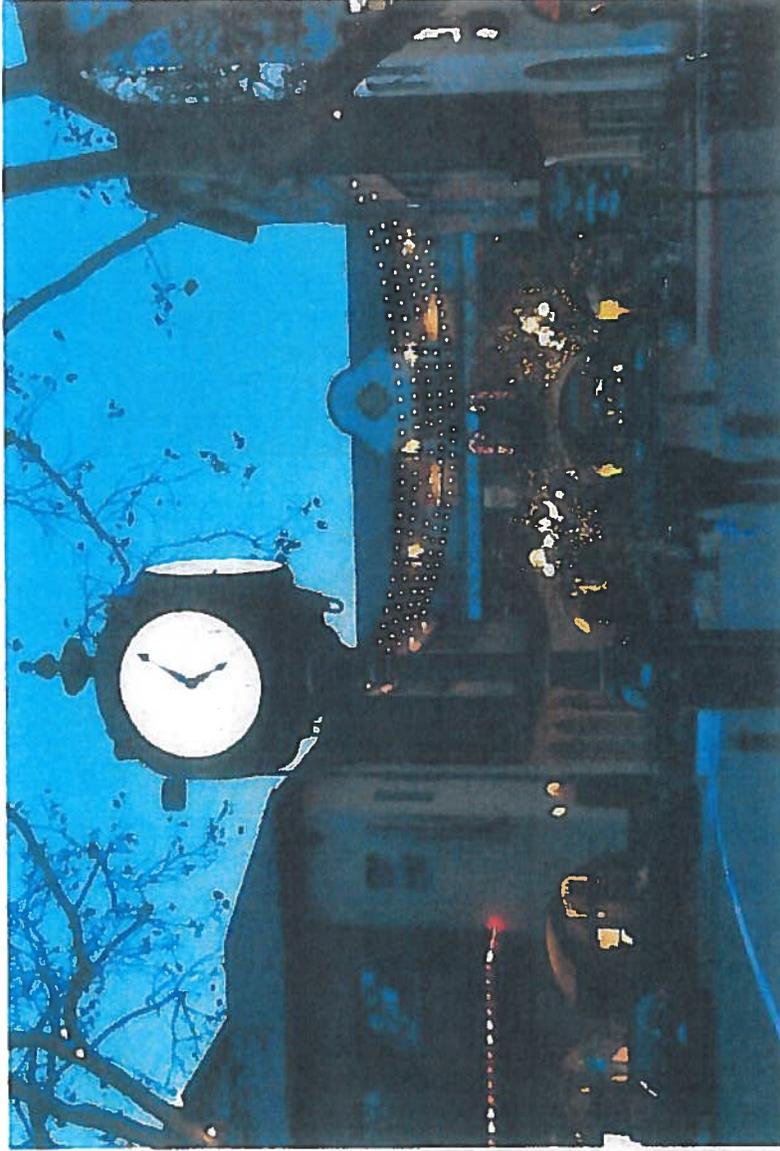
DOWNTOWN CONCORD SPECIFIC PLAN

4.2 URBAN FORM, MASSING AND CHARACTER

The urban form around Todos Santos is defined by buildings ranging from low rise/single story to three stories and has active ground floor uses that support the activity and vitality of the park. Higher density office commercial is predominantly situated near the BART station and Clayton Road. These tall buildings provide a sense of skyline to the City, become an important commercial focus and surround Todos Santos Plaza on three sides, creating a low rise/pedestrian center to the City.

The project area is characterized by a strong square/rectangular street grid that is highly walkable. In areas where the grid has been aggregated into larger blocks to accommodate higher density and larger footprint buildings (such as at the Park & Shop Shopping Center and near the BART Station), pedestrian walkability and accessibility decrease, creating a strong disconnection from the surrounding area. It is notable that the Central Business District and Retail Center have a strong correlation between generally larger building footprints and larger block size, whereas the residential districts and downtown core exhibit smaller buildings and smaller block sizes and increase walkability.

The project area is marked by a number of historic buildings that exemplify early central California architecture. Their character is defined not only stylistically, but through key elements that the following design guidelines will illustrate. These generally include but are not limited to the following:

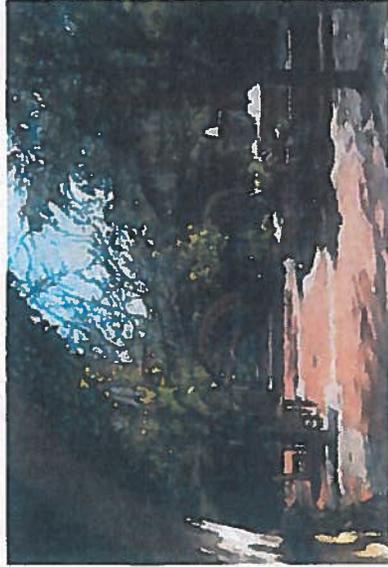


Todos Santos Plaza looking toward Salvio Pacheco Square

- Breaking up single large block buildings into a smaller series of buildings/variation in the facades to create a finer building grain fabric
- Providing important roofline articulations/stepping back the top floors of buildings
- Ensuring the ground floor of buildings relate and enhance the public realm/streetscape
- Providing deep reveals for window treatments
- Incorporating balconies with permeable railings
- Use of a common materials palette
- Use of arcades along specific streets

TODOS SANTOS PLAZA

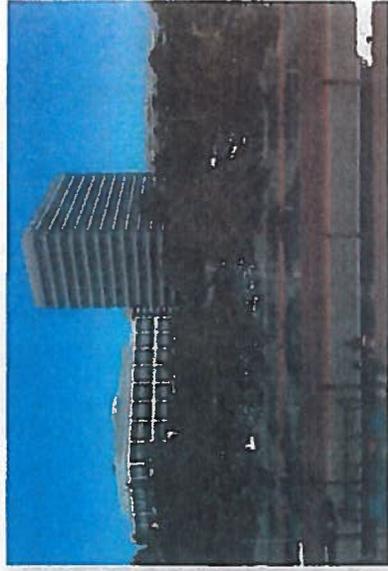
The building form and fabric around Todos Santos Plaza and the adjacent area helps define the pedestrian character of the downtown. The small scale and fine grain fabric is an asset that new infill development should follow.



Salvio Pacheco

BART STATION AREA

The BART station and associated track creates a significant divide within the urban form of the city, where higher density commercial programs exist on the northern side, while single family residential exists to the south.



BART Station Area

The larger parcels around the BART station allow for higher density development, but the proposed density should be appropriately massed, including stepping back the top floors of buildings, creating variation within facades of buildings, and creating strong visual building breaks.

High quality architecture should be expected here, as this is a major gateway into the City and the Downtown. Buildings that surround the proposed BART plaza should be planned and designed in a cohesive manner, with entries and ground floor uses that are accessible to pedestrians and transit users.

PARK & SHOP AREA

As this area redevelops, establishing a building form that promotes pedestrian activity, is inviting and is built at a pedestrian-scale will be critical to ensuring success for this area. Allowing buildings to relate to the street will be important, as well as promoting a strong streetscape to help modulate the speed of traffic along Willow Pass and Clayton Road.



Existing Park + Shop parking and retail

4.3 DESIGN GUIDELINES

BUILDING SETBACK

Intent

Buildings on side and cross streets can create a more intimate scale and help hold the street volume. Setbacks on these streets are not desired except in the case of residential streets or ground floors with residential use where a private zone between public and private areas is desired. The following guidelines elaborate these conditions.

Buildings with minimal setbacks have a special relationship with the sidewalk and street. In these cases, buildings frame the street and form a well-defined street edge. Activities within the building, if seen, particularly at ground level, can provide visual interest and a degree of safety to passersby. Activities outside the building, such as outdoor dining, can enliven adjacent sidewalks. These are desirable attributes in areas with high levels of activity such as the downtown and station area.



Minimal setback and transparent facade, Sacramento, CA



Pasadena Old Town



Landscaped setback and raised entry provides privacy

Guidelines

Buildings located on all streets in the Project Area shall maintain setbacks as required by the City Zoning Codes.

Steps, stoops, porches, patios, and terraces should be allowed in the building setback zone, particularly on residential buildings where ground floor activity is important.

In residential neighborhoods with low scale buildings, the front setback shall be landscaped to enhance the street character and complement the neighborhood identity.

In case of corner lots in residential neighborhoods, the minimum permeable surface of the combined area of the front and street sideyard setback zones facing the streets should be 75 percent. These setbacks shall be landscaped to enhance the street character.

When possible, ground floor retail space should be setback a minimum of 2 feet and a maximum of 5 feet to provide for outdoor seating opportunities by way of wider sidewalks.

Building setbacks should be landscaped to ensure privacy in case of residential ground floor use.

All new developments on primary streets should build to zero front lot line with exceptions for any usable, publicly-accessible, at grade open space such as small plaza, pocket park, or a pedestrian alley.

Buildings should be sensitive to the scale and character of adjacent buildings on rear property lines.

Buildings should complement the low-scale, horizontal character of the Specific Plan area, and ensure a basic horizontal articulation, by differentiating the ground floor from the upper floors or roof.

Buildings should consider sun shading as part of a modulation and articulation strategy.

Buildings downtown should maintain a tight and varied rhythm of façades compatible with the existing character. In particular, they should relate to the typical 50 foot wide parcel width through building vertical modulation and façade articulation to avoid flat, long walls along the street frontage. Such techniques could include the use of change in façade rhythm, façade recesses, or change in materials or color.

DOWNTOWN CONCORD SPECIFIC PLAN

GROUND FLOOR TREATMENT

Commercial Ground Floor

Intent

Active, pedestrian-oriented, inviting ground floor retail is an essential component in the creation of a vibrant district and neighborhood.

Guidelines

All ground floor retail should have a primary entry from the sidewalk or from a forecourt or courtyard that has direct access to a sidewalk.

Individual storefronts shall be clearly defined by architectural elements, such as piers and separations of glass.

Commercial buildings are recommended to meet the sidewalk with an interactive ground floor use, or a transition landscaped setback, or a pocket plaza, to contribute positively to the pedestrian experience.

Ground floor retail with multiple tenants should be designed to have clear distinction between individual storefronts, entire building façade, and adjacent properties.

For larger retail tenants, entries should generally occur at a minimum of every 50 feet. In-line retail stores should generally have entries every 25 feet.

Recessed doorways for retail uses are allowed, and they should be a minimum of 2 feet in depth. Recessed doorways provide cover for pedestrians and customers in bad weather; they help identify the location of store entrances, provide a clear area for out-swinging doors, and offer the opportunity for interesting paving patterns, signage, and displays.

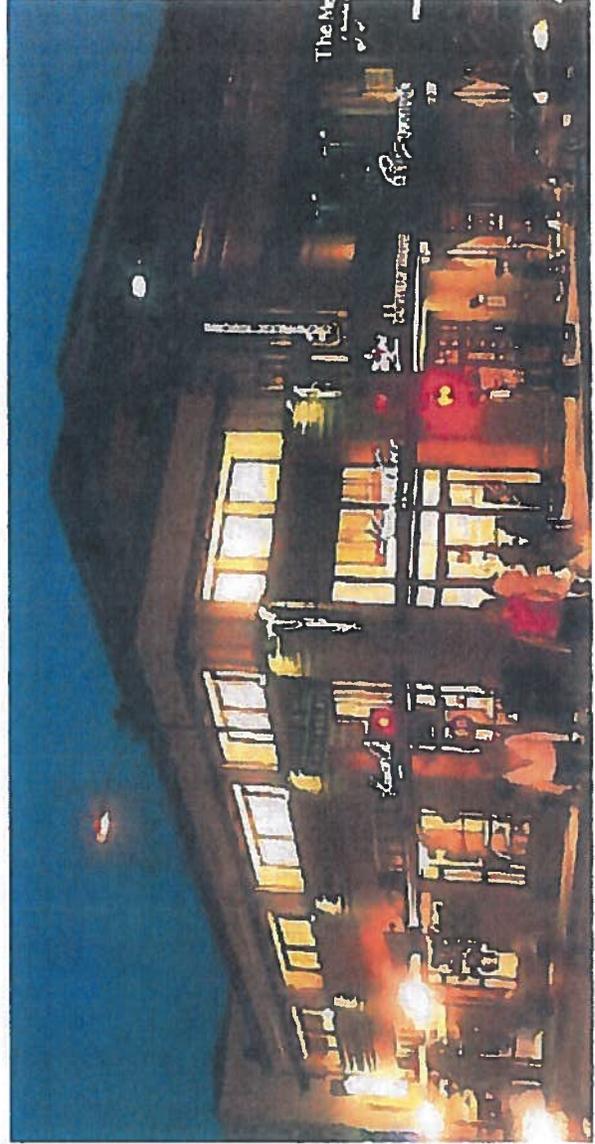
Retail frontage, whether ground or upper floor, must be clear vision glass; no heavily tinted or mirrored glass is permitted.

Storefronts should remain un-shuttered at night and provide clear views of interior spaces lit from within.

Storefront windows should provide deep merchandising zones that allow for changeable and dimensional displays. The windows should not be completely obscured with display cases that prevent customers and pedestrians from seeing inside.



Boutique retail alley



Transparent retail facade

Residential Ground Floor

Intent

The character of the building's ground floor determines the overall quality of the street level pedestrian experience. Residential ground floor use adds vibrancy and life at the street and plaza level and ensures "eyes" on the street as an important aspect for public safety.

Guidelines

Units located at ground level shall have their ground floor elevated a minimum of 18 inches above the street level for privacy, provided that local accessibility codes are met.

Internal active uses, such as community rooms, fitness center, daycare facilities and sales centers, should be placed at the ground level along the street.

Multiple entries at street level are encouraged where possible.

Direct-access residential units are recommended on primarily residential streets.

Stoops and landscaping create inviting, usable transition spaces. Stoops and entry steps from the street are encouraged for individual unit street entries, consistent with local accessibility requirements. Stoops could extend in to the building setback zone but shall not encroach into the public right-of-way.

For larger residential buildings with shared entries, entry should be through prominent entry lobbies or a courtyard facing the street. Setback at entries is encouraged.

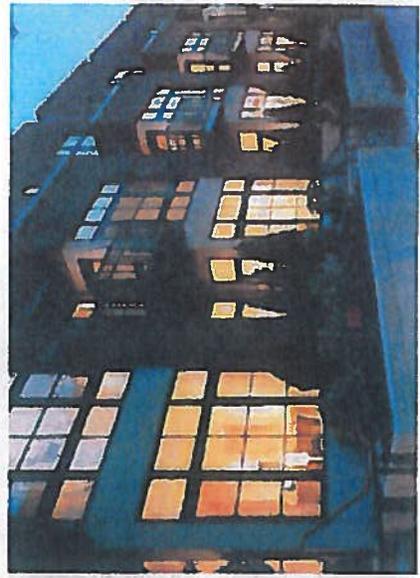
Entries should be prominent and visually distinctive from the rest of the façade with creative use of scale, materials, glazing, projecting or recessed forms, architectural details, color, and/or awnings.

Breaks in the ground floor for vehicular and service entries should be minimized.

Multi-unit residential buildings are encouraged to introduce openings along the public street that provide visual or physical access to courtyards. Such openings add an element of surprise and interest at the street level.



Wisteria development, Concord, CA



Townhomes



Townhomes

Blank Wall Treatment

Intent

Treatment of blank walls should ensure pedestrian comfort, safety and interest.

Guidelines

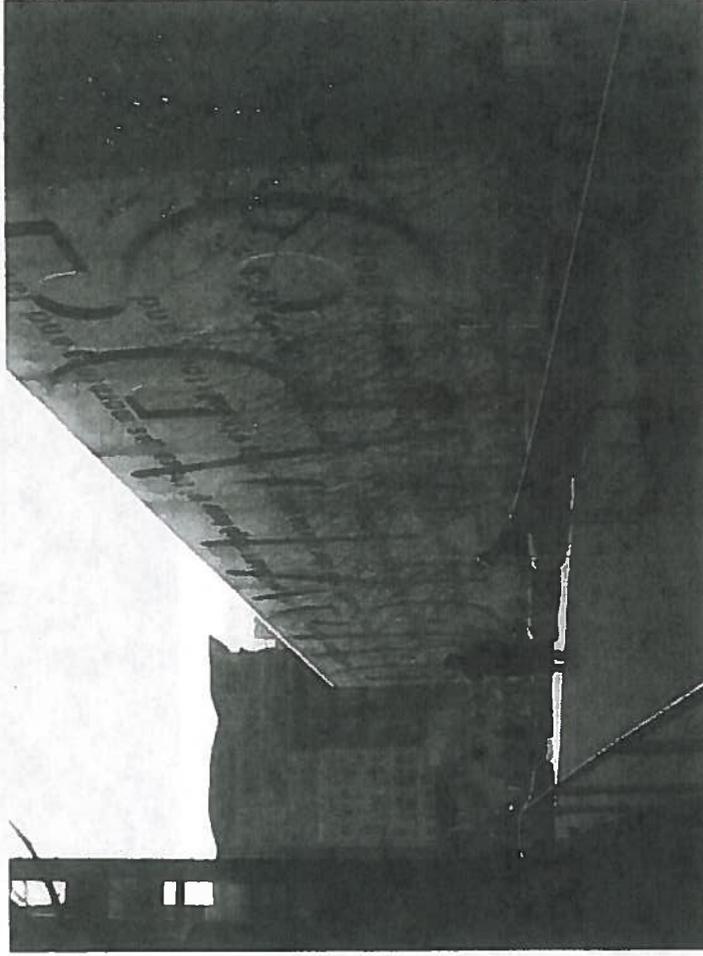
Unavoidable blank walls enhanced with architectural detailing, material texture, landscape treatment or art work shall be no longer than a maximum length of 50 feet.

Unavoidable blank walls along public streets, besides being detailed, shall be provided with additional special lighting to ensure safety and comfort during night time.

Blank wall including solid doors should be avoided wherever possible.

Unavoidable blank walls on the ground floor along public streets and open spaces should be treated to create a pleasant visual experience. This treatment could be in the form of either:

- installing vertical trellis in front of the wall with climbing vines or plant materials,
- setting the wall back and providing a landscaped or planter bed in front of the wall, including plant materials that could grow to obscure or screen the wall's surface
- providing art (mosaic, mural, decorative masonry pattern, sculpture, relief, etc.) over a substantial portion of the blank wall surface,



Sculptural facade treatment

- employing indentations, or other means of breaking up the wall's surface,
- providing a canopy, horizontal trellis or other pedestrian-oriented features that add visual interest

Vehicular Access

To minimize disruption to primary pedestrian-oriented streets, garage entrances should be generally located in alleys or side streets.

Garage entrances adjacent to sidewalk should be screened with landscaping techniques or should be treated as an opportunity for public art (Figure 3.51).

In mixed-use developments, in order to minimize curb cuts, shared ramps for both retail and residential uses are encouraged. In shared ramp conditions, secure access for residential parking should be provided.

In order to minimize curb-cuts, multi-unit residential buildings should consolidate their parking entries and exits to a single entry.



Santa Monica Civic Center Parking Garage facade

DOWNTOWN CONCORD SPECIFIC PLAN

Residential/Mixed Use Building Design

Intent

Residential/mixed use buildings represent the largest amount of new program in the Project area. Therefore these new buildings should conform to key aspects of massing, pedestrian scale and promotion of ground floor usage. These elements are essential to creating a livable and vibrant Downtown.

Guidelines

Multi-unit buildings should depict a rhythm and scale that relates to the surrounding buildings. In case of adjacent buildings being smaller in scale, such as single-family units, the multi-unit buildings should maintain the scale of the adjacent buildings on the street front. It should place the bulk of the building mass away from the street towards the center of the block.

Multi-unit buildings should have modulation in massing to avoid a box-like structure. Creating terraces, recessing windows and use of step backs create distinct smaller volumes.

Multi-unit buildings should articulate their facade to avoid a flat, monotonous appearance. Use of projecting building elements, windows or balconies helps break the façade and reduce the apparent size of the building.

Primary facades of new buildings should be compatible with surrounding buildings in relation to the width and proportions of elements like front porches, stoops, overhangs, projected components and roofs.

Roofs should be treated as the fifth façade of the building since they play a major role in the appearance and character of a building. Level changes of the roof help soften the mass of the building.

The scale, proportions and placement of the architectural details on all new building facades should be compatible with the overall aesthetics of the surrounding buildings.

Buildings on corner lots should articulate both their street-facing facades. Facade treatment and openings on both these exposed surfaces should be designed to optimize the greater street visibility and accessibility to sunlight and air.

Wherever possible, corner lot buildings are encouraged to include a corner entry.



Paseo Chapala, Santa Barbara, CA



Residential building reflecting early California architectural style



Mixed-Use Housing, Santa Monica, CA

Commercial Building Design

Intent

Large commercial buildings should be detailed to integrate well in its surrounding context.

Guidelines

Commercial buildings with a large mass should be broken down in to smaller distinct volumes to avoid a box-like structure.

Long, continuous facades should be articulated with architectural elements and wall plane projections or recesses to reduce the massive scale and uniform physical appearance.

Expression of the structural elements and bays of the building on the façade is encouraged. Windows, wall panels, and pilasters should be based on a module derived from the building's structural bay spacing.

Street-level frontage adjacent to public streets or open spaces should be articulated with entrances, lobbies, storefront windows and displays to enliven the public realm experience.

Commercial buildings are encouraged to have variations in rooflines to enhance the distinct massing.

Mechanical Equipment

Mechanical equipment on top of the buildings should be screened from both pedestrian and adjacent rooftop views. The screen should be designed to be architecturally integrated as part of the roofscape or the building facade.

Intensive or extensive green roofs that help reduce storm water run-off should be explored for all rooftops.

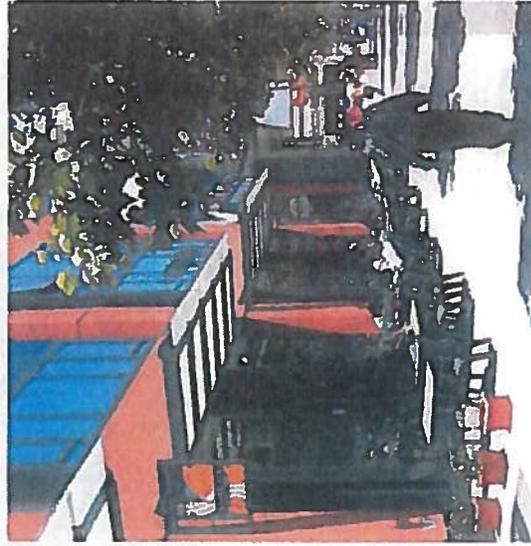
Utilities

All utilities in conjunction with new residential and commercial development should be placed underground.

Above ground meters, boxes and other utility equipment should be screened from public view through use of landscaping or by integrating into the overall building design



Facade articulation, Portland, OR



Facade articulation, San Francisco, CA

DOWNTOWN CONCORD SPECIFIC PLAN

4.4 PARKING AND SERVICING

PARKING STRUCTURES AND GARAGE ENTRANCES

Intent

Due to their scale and treatment, parking structures are very often a disruptive element in the urban fabric. It is important to locate and access parking structures and residential garages such that the overall pedestrian flow and experience on the public streets is not compromised.

Parking podiums and below ground parking are encouraged as a way to screen large volumes for parking for residential and commercial developments.

Guidelines

Parking structure lighting shall provide adequate security, but openings shall be screened and controlled so as not to disturb surrounding residences and streets from garage lighting at night.

Gates for podium parking/parking garages should be opaque and match the building in terms of aesthetic character

Parking garage driveways should not be placed on major pedestrian streets (e.g. Grant Street)

Parking garages and surface parking areas should be screened from pedestrian areas (streets and open spaces) with landscaping, liner uses such as retail, lobbies, community uses, or residential units. All service areas must be screened and not placed along major pedestrian streets or access ways.

Surface parking should be visually attractive, address security and safety concerns, retain existing mature trees and incorporate canopy trees for shade.



Kettner rowhouse, San Diego, CA



Magnolia Row driveway, Oakland, CA

4.5 PRIVATE OPEN SPACE

The provision and treatment of private open space on individual parcels can enhance the character of public streets and sidewalks and private development. It can add to available public open space in the area.

The Specific Plan encourages use of the following guidelines when incorporating open space in private developments.

Guidelines

Private and/or common open spaces are encouraged as part of building modulation and articulation to enhance building facades.

Private developments should provide accessible and usable common open space for building occupants and/or the general public.

For residential developments, private open space should be designed as an extension of the indoor living area, providing an area that is usable and has some degree of privacy.

Landscaping in setback areas should define and enhance pedestrian and open space areas. It should provide visual interest to streets and sidewalks, particularly where building facades are long.

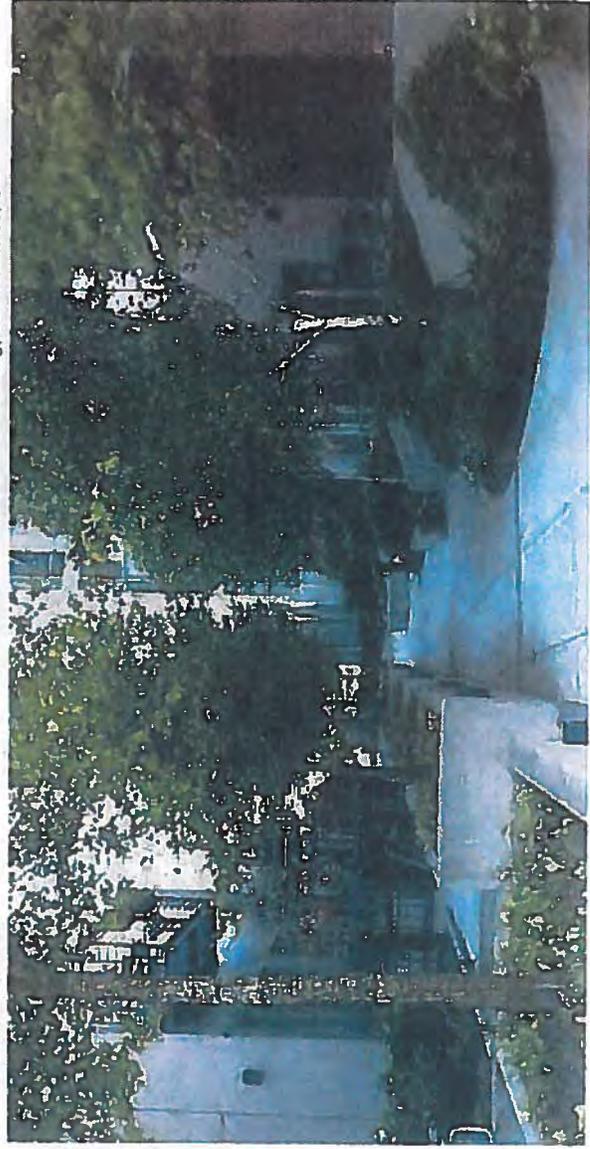
Landscaping of private open spaces should be attractive, durable and drought resistant (see Section Sustainable Practices-Landscape Guidelines for details)



Mid-block access, Portland, OR



Mission Creek housing, San Francisco, CA



Shared courtyard, Portland, OR

DOWNTOWN CONCORD SPECIFIC PLAN

4.6 SUSTAINABLE PRACTICES

Sustainable practices for new construction support community and environmental well-being by utilizing finite resources in a responsible way, creating healthy environments for building inhabitants and minimizing impacts to both natural systems and existing utilities (i.e. water, wastewater and energy systems). The City of Concord supports sustainable practices through its 2013 Climate Action Plan.

Sustainable practices address: 1) the environmental impacts of site development and building construction; and 2) the long-term environmental impacts of the operation of buildings resulting in the emission of greenhouse gases (GHGs), in particular carbon dioxide (CO₂), which is causing the global climate to change. Currently, there are excellent tools to measure ways to reduce environmental impacts caused by building construction, and new tools are emerging to measure greenhouse gas emissions caused by building operations over the long term.

To address impacts caused by construction, the U.S. Green Building Council's (USGBC) Leadership in Energy and Environmental Design (LEED) rating system measures specific site development and new building construction methods related to environmental issues, such as energy savings, water efficiency, CO₂ emissions reduction, improved indoor environmental quality and stewardship of resources and sensitivity to their impacts.

To address GHG emissions, the world's leading green building organizations have agreed to adopt a common global language for the measurement of the carbon footprint of buildings. The "common carbon metric" will be piloted by the leading green building rating tools. This should lead to the cost-effective GHG mitigation potential of buildings, which account for around 40% of the world's energy use and 33% of global GHG emissions.

MEASUREMENT TOOLS

Development and Construction Tools

The LEED program has performance levels from "Certified" to "Platinum" and rating systems that address different types of construction and building operation, including LEED for Neighborhood Development, LEED for New Construction, and LEED for existing buildings, operations and maintenance. Many municipalities in the Bay Area have adopted Green Building Ordinances that require certain levels of LEED certification for different types of projects.

The Specific Plan proposes that all new development in the Project Area meet LEED Silver Standards



Stormwater management and green street

Greenhouse Gases/Carbon Tools

The 2030 Challenge is an initiative by Edward Mazria and Architecture 2030 asking the global architecture and construction community to adopt a series of greenhouse gas reduction targets for new and renovated buildings. In response to the global-warming crisis, the 2030 Challenge's mission is to rapidly transform the US and global Building Sector from the major contributor of greenhouse gas emissions to a central part of the solution to the global-warming crisis.

The "carbon metric" measurement device is currently being developed and will be integrated into the LEED program in the future.

Initiatives

Local and regional initiatives address sustainable development and reduction of greenhouse gases.

Local Initiatives

The City of Concord published a Climate Action Plan (CAP) in 2013. The CAP includes recommendations for environmentally responsible development and ways to reduce greenhouse gas emissions. The CAP's recommendations for sustainable building and development practices refer to a phased program for submittals of Green Building Checklists related to development projects. The CAP also recommends early adoption of the California Green Building Code.

Guidelines

LEED certification, at a silver level or higher, should be required for the types of projects listed below. The applicable LEED® versions of performance standards are: LEED®- v3 (2009) New Construction; LEED®- v3 (2009) Core and Shell; LEED®- v3 (2009) Schools; and LEED®- v3 (2009) Commercial Interiors. LEED certification, at a silver level or higher, should be required for:

Newly constructed (Residential) occupancy buildings with three or more dwelling units;

- Newly constructed commercial buildings occupancies including among others office, professional and service type transactions and occupancies including among others display or sale of merchandise such as department stores, retail stores, wholesale stores, markets and sales rooms) that are 5,000 gross square feet or more;
- New first-time build-outs of commercial interiors that are 20,000 gross square feet or more in buildings of Group B and M occupancies; and
- Major alterations that are 20,000 gross square feet or more in existing buildings of where interior finishes are removed and significant upgrades to structural and mechanical, electrical and/or plumbing systems are proposed.

- Because the development of larger parcels provides the ability to incorporate cost effective carbon reduction features and renewable energy sources, development projects over 4 acres of land should have more stringent sustainability requirements and GHG reduction targets. These could include being certified at a LEED ND (neighborhood development) level of gold, and mandating a phased reduction of GHG emissions over a period of time, such as those prescribed in the 2030 Challenge.
- Because green building standards are constantly evolving, the requirements in this section should be reviewed and updated on a regular basis of at least every two years.

DOWNTOWN CONCORD SPECIFIC PLAN

Solar Access Guidelines

Building design should consider floor-to-ceiling height and floor plan depth to allow natural light deeper into the interior.

Buildings should reduce use of daytime artificial lighting through design elements, such as bigger wall openings, light shelves, clerestory lighting, skylights, and translucent wall materials.

Buildings should allow for flexibility to regulate the amount of direct sunlight into the interiors.

Louvered wall openings or shading devices like bris soleils help control solar gain and check overheating. Bris soleils, which are permanent sun-shading elements, extend from the sun-facing facade of a building, in the form of horizontal or vertical projections depending on sun orientation, to cut out the sun's direct rays, help protect windows from excessive solar light and heat and reduce glare within.

Where appropriate, buildings should incorporate arcades, trellis and appropriate tree planting to screen and mitigate south and west sun exposure during summer. This guideline would not apply where buildings have a minimum setback and street trees provide adequate shade.

To maximize use of solar energy, buildings should consider integrating photovoltaic panels on roofs.

Stormwater and Wastewater Management Guidelines

Buildings should incorporate intensive or extensive green roofs in their design. Green roofs harvest rain water that can be recycled for plant irrigation or for some domestic uses. Green roofs are also effective in cutting-back on the cooling load of the air-conditioning system of the building and reducing the heat island effect from the roof surface.

Projects should use porous material on driveways and parking lots to minimize stormwater run-off from paved surfaces.

Effective stormwater management techniques are recommended. Such techniques could include bioswales on surface parking lots and rain gardens in landscaped areas.



Roof photovoltaics, light shelf and green roof



Street lighting examples

Landscaping Guidelines

Planting plans should support passive heating and cooling of buildings and outdoor spaces.

Regional native and drought resistant plant species are encouraged as planting material.

Provision of efficient irrigation system is recommended, consistent with the City's Municipal Code Chapter 12.44 "Water-Efficient Landscaping".

Lighting Guidelines

Energy-efficient and color-balanced outdoor lighting, at the lowest lighting levels possible, are encouraged to provide for safe pedestrian and auto circulation.

Glare into dwelling units and light pollution into the night sky should be minimized by use of fixtures with low cut-off angles.

Improvements should use ENERGY STAR-qualified fixtures to reduce a building's energy consumption.

Installation of high-efficiency lighting systems with advanced lighting control, including motion sensors tied to dimmable lighting controls, are recommended.

Green Building Material Guidelines

The reuse and recycle of construction and demolition materials is recommended. The use of demolition materials as a base course for a parking lot keeps materials out of landfills and reduces costs.

The use of products with identifiable recycled content, including post-industrial content with a preference for post-consumer content, are encouraged.

Building materials, components, and systems found locally or regionally should be used, thereby saving energy and resources in transportation.

Layouts with adequate space to facilitate recycling collection and to incorporate a solid waste management program, preventing waste generation, are recommended.

The use of material from renewable sources is encouraged.



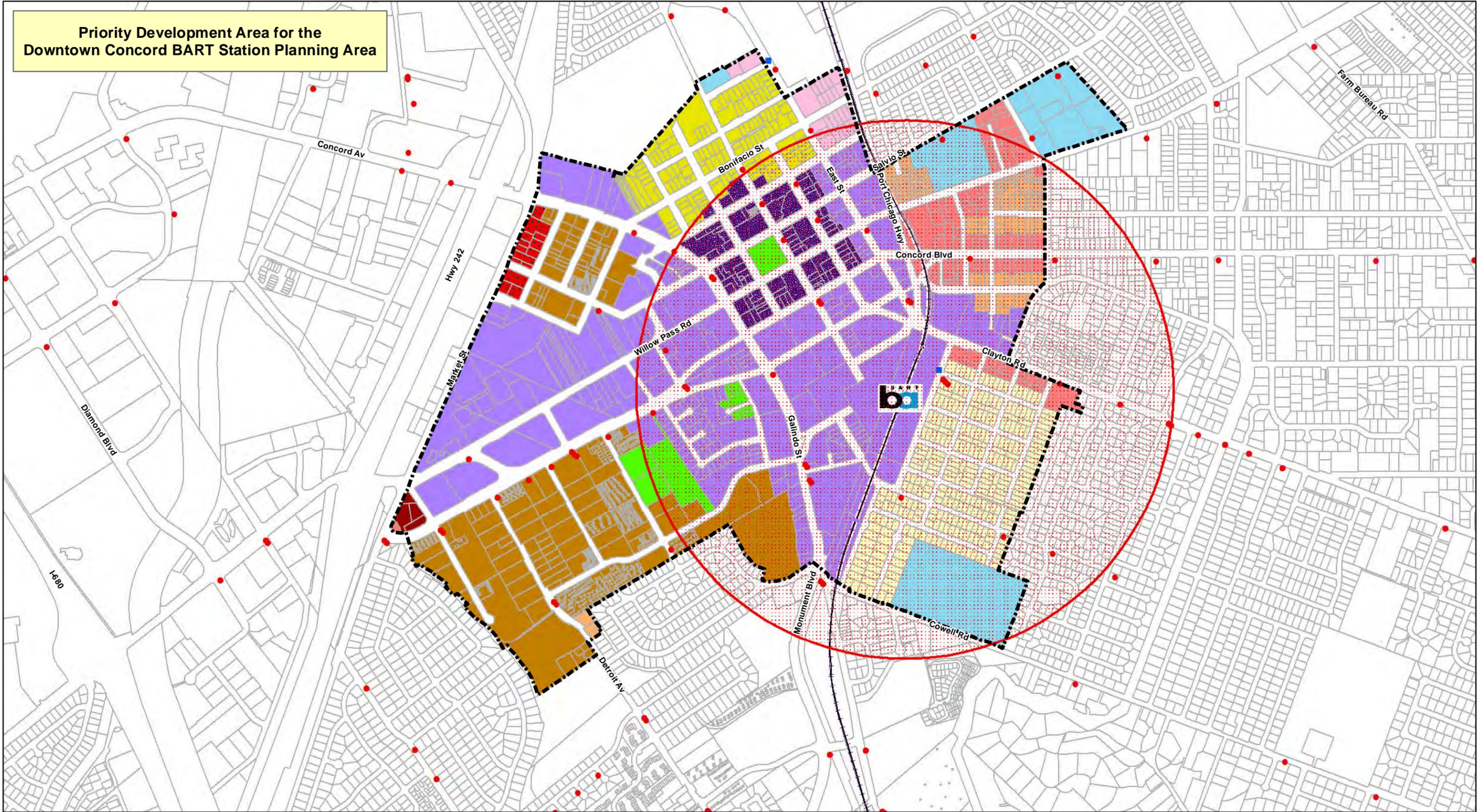
Fig 3.11 Specific Plan Phase I Building Use

DOWNTOWN CONCORD SPECIFIC PLAN



Fig. 3.12 Illustrative Plan - Phase 1

**Priority Development Area for the
Downtown Concord BART Station Planning Area**



Disclaimer.
Although every reasonable effort has been made to assure the accuracy of this data, the City of Concord makes no warranty, representation or guaranty as to the content, sequence, accuracy, timeliness or completeness of any of the data provided herein and explicitly disclaims any representations and warranties, including, without limitation, the implied warranties of merchantability and fitness for a particular purpose. The City of Concord assumes no liability for any errors, omissions, or inaccuracies in the information provided regardless of how caused and assumes no liability for any decisions made or actions taken or not taken by the user of the data in reliance upon any information or data furnished hereunder. Because the GIS data provided is not warranted to be up-to-date, the user should check with the City staff for updated information.

Legend

- PriorityDevelopmentAreaNew
- Half Mile Radius of BART
- BART Station
- MTC Bus Data**
- Bus Stop by Agency**
- The County Connection
- TriDelta Transit

GP 2030

- Rural Residential (RR)
- Low Density Residential (LDR)
- Medium Density Residential (MDR)
- High Density Residential (HDR)
- North Todos Santos (NTS)

- Community Office (CO)
- Commercial Mixed Use (CMU)
- Neighborhood Commerical (NC)
- Regional Commerical (RC)
- Service Commerical (SC)
- Downtown Pedestrian (DTPD)

- Downtown Mixed Use (DTMU)
- West Concord Mixed Use (WCMU)
- Business Park (BP)
- Industrial Mixed Use (IMU)
- Heavy Industrial (HI)
- CRP neighborhood and village districts

- CRP non-residential development districts
- CRP TOD districts
- CRP open space
- Military (MIL)
- Public/Quasi-Public (PQP)
- Hospital/Medical Center (PQPHMC)

- Open Space (OS)
- Parks and Recreation (P)
- Rural Conservation (RCON)
- Wetlands/Resource Conservation (WRC)
- Unclassified (U)



DESIGN GUIDELINES FOR THE NORTH TODOS SANTOS NEIGHBORHOOD

PURPOSE

The North Todos Santos Neighborhood is comprised largely of residential structures which date from a fifty-year period of 1880-1930. Architectural styles include Italiante, Queen Anne, Craftsman, Bungalow, Mission, and others, mostly applied to houses of modest scale.

The purpose of these guidelines is to assure that new development of multi-family and office buildings will fit comfortably into the neighborhood without overwhelming the older buildings. It is desirable to see new buildings blend into the existing environment. The late 19th- and early 20th-century buildings will set the norm. However, the guidelines are not intended to suppress good contemporary design using appropriate and economical material, nor are they intended to encourage cheap historical imitations. A statement from the "Todos Santos Design Vocabulary" (which was developed for areas to the south of this neighborhood) is appropriate here also:

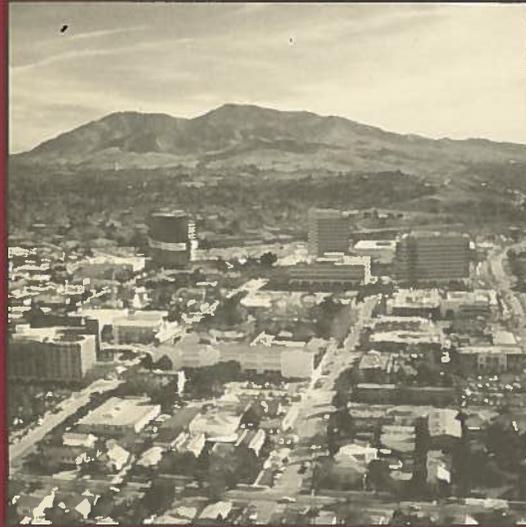
"Absolute uniformity, nor historical re-creation of a singular style, is not the aim. Quite the opposite. It is a highly contextual aim--to weave or knit together old and yet-to-come buildings into a whole environment."

GUIDELINES

1. Building Mass -
 - a) Large projects should be articulated in units of residential scale, either as separate buildings or as distance wing elements; residential scale will be particularly important near site boundaries and street.
 - b) Wall lengths greater than 50 feet should be interrupted by projections, indentations, or changes of direction.
 - c) Effects of relatively high, non-residential floor-to-ceiling heights should be offset by "scale-giving" elements such as porches, trellises, window projections, and lowered cornice or fascia lines.
2. Roof Forms -
 - a) Gable or hip roofs are most appropriate. Complex forms, such as combinations of gables and hips, may help reduce the apparent scale of large buildings.

- b) Mansard, shed, and flat roofs are discouraged.
 - c) On street facades of multi-story buildings, it will generally be appropriate for the roof to slope away from the street in order to reduce the apparent mass.
3. Windows - Especially along street facades and other highly visible walls, windows should be used as sculptural elements, with expressed sills, lentils, or other trim. Such historical elements as bay windows and French doors may be used to good effect.
4. Building Entries - Entries should be used as important design features, to suggest "welcoming, domestic" qualities. Residential scale should be maintained in the design of entry features.
5. Decorative Elements -
- a) Details such as mouldings, brackets, projecting rafters, etc., are encouraged. Inspiration should be drawn from examples of buildings existing in the neighborhood.
 - b) Elements and materials should be appropriately scaled to the mass of the proposed building.
6. Landscaping -
- a) Landscape materials and arrangements should reflect a traditional residential character.
 - b) Landscaping will extend to the curb line. Shade trees will be required along the street edge.
 - c) Where parking areas are exposed to view from a street, shrubbery will be used to minimize the view of parked cars and pavement. Parking areas will be required to be screened along all property line perimeters using trees, shrubs, or vines as appropriate.

DOWNTOWN
CONCORD



URBAN DESIGN

M A R C H 1 9 8 7

CONCORD CALIFORNIA

DOWNTOWN
CONCORD



URBAN DESIGN



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Todos Santos Plaza with The Fire House in the background at Grant Street and Willow Pass Road.

I NTRODUCTION

“Downtown Concord Urban Design” is a book dedicated to the achievement of excellence in the environment of downtown Concord, California. It presents ideas about the character of downtown, policies for urban design, and procedures for review of downtown building projects. It is to be used by the City’s elected officials, professional staff, and public Commissions and Boards. And it is to be used by private developers, institutional sponsors, and individuals who have an interest in the quality of Concord’s downtown environment.

The book addresses the physical environment on several levels: generally and specifically, quantitatively and qualitatively. Urban design issues are depicted on maps, discussed in text, and illustrated in photographs and drawings. The book considers issues such as the form and texture of public streets, sidewalks, and open spaces. It distinguishes the predominantly pedestrian from the predominantly vehicular environments. It talks about the location of strategic activities, such as retail shops, offices, hotels, cultural facilities, and housing. It provides guidance for shaping open space and treating landscape. It proposes specific and general design guidelines for the architecture of buildings in the downtown.

The book is intended to be a guide to provide direction and it is intended to be modified as conditions warrant. It is not a specific plan, a zoning ordinance, or a building code (information concerning these and other legal documents of the City of Concord can be obtained upon request from the appropriate administrative departments of the City). It invites change and refinement as experience with its use suggests new concepts or invalidates old ones. It does not require development to occur at a specified rate, but provides guidance for change at whatever rate the City deems appropriate.

Perhaps the most important element of the book is that it suggests an image for the future of downtown Concord. Through the numerous illustrations, maps, notes, and guidelines, the book creates a perceptible and unique urban character, which will become evident through the cumulative effect of completed downtown improvements.

DOWNTOWN CONCORD

In recent years, downtown Concord has been changing from a small town center within a suburban community to a small urban center within the growing Contra Costa County region. Because of Concord's location and role within the central Contra Costa County area, the trend will probably be for the downtown to continue to change in the decades to come. This change necessitates new perceptions of the character of downtown. What should this character or image be?

Until now, Concord's image has been that of a small town. The form of downtown has been defined by small scale buildings, low site utilization, and architectural idioms from Victorian and Spanish Colonial influences. A new downtown must keep what is good and familiar from the past, while permitting and incorporating new ingredients.

A new image calls for a new interpretation of what a small California city can be. America's urban prototypes (e.g. 19th century industrial cities such as San Francisco, Chicago, or Cleveland) are not appropriate as models for Concord's future. A new image for Concord can be found in an emerging California urbanism which responds to lifestyles of the western United States, the climate of California, the post-industrial information age, and the history and geography of Concord. Many people in Concord have expressed their conceptualizations of this new urbanism in the numerous public forums that were held during the planning and preparation of this book.

The new image will have a downtown that will be green with trees, lawns, and flowering plants. Todos Santos Plaza, its neighboring blocks, and the Bank of America office campus will provide extensive landscaped pedestrian spaces on eight

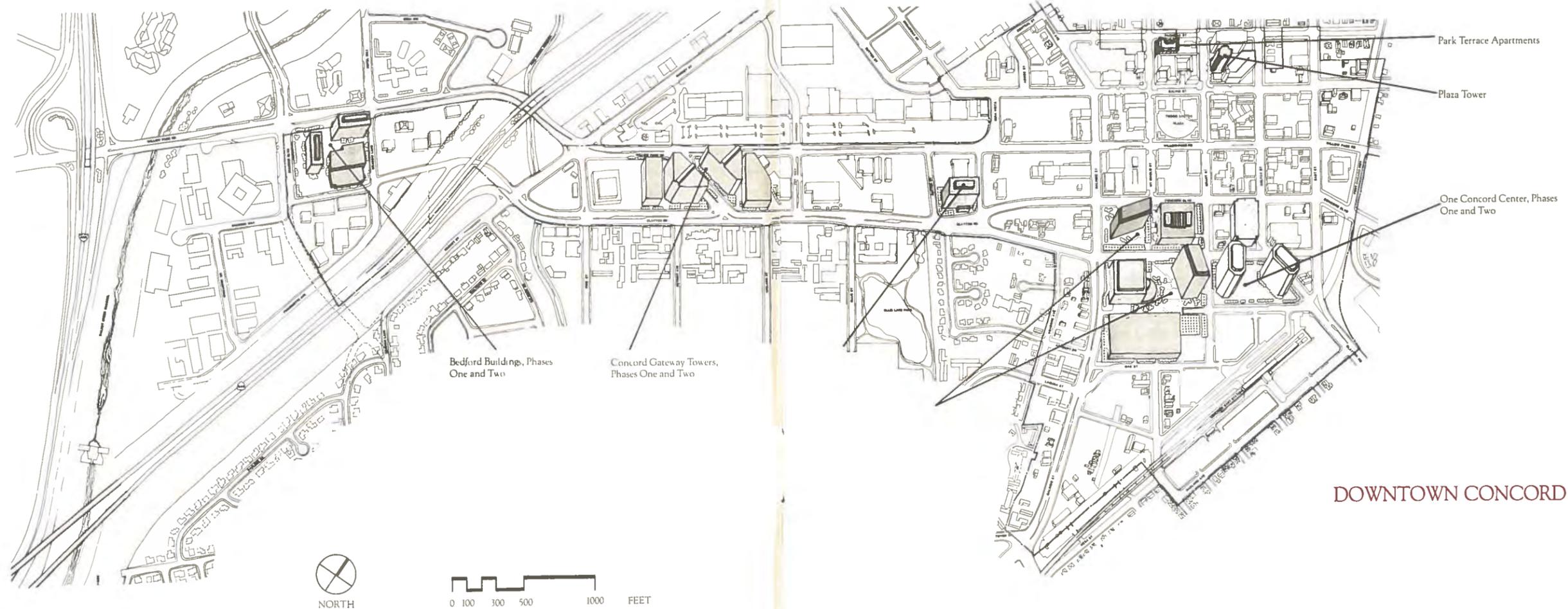


The plan depicts a pedestrian environment at the core of downtown, bordered on most of three sides by higher density buildings.

city blocks in the downtown core. These green spaces will be defined by continuous colonnades at the street level of buildings along Grant, Salvio, and Galindo Streets. Grant Street will be specially designed for pedestrians, with limited vehicular access connecting Todos Santos Plaza to the BART station. It will be lined with shops and cafes and will feature outdoor art and fountains.

Buildings around Todos Santos Plaza will remain low in height. Taller structures will be clustered around the BART station. New and old buildings will house shops, restaurants, hotels, offices, theaters, and housing in an exciting downtown with a relaxed and humane atmosphere.

The map shows a view of downtown Concord in 1985. It registers the image of downtown at a point in time, to be used as a yardstick for comparison with the transitions of the downtown environment in later years.



DOWNTOWN CONCORD

4. Grant Street is a major pedestrian promenade extending from Todos Santos Plaza to the BART station. It has special paving on sidewalks and at street intersections. Custom designed lighting, street furniture, and public art will provide a festive character. The street will be lined with retail and restaurant activities. Short-term, on-street parking can act as a buffer between wide sidewalks and relatively narrow streetbeds. Grant Street could be the route of a public shuttle service utilizing a small jitney or streetcar, although it will not be a transit mall.

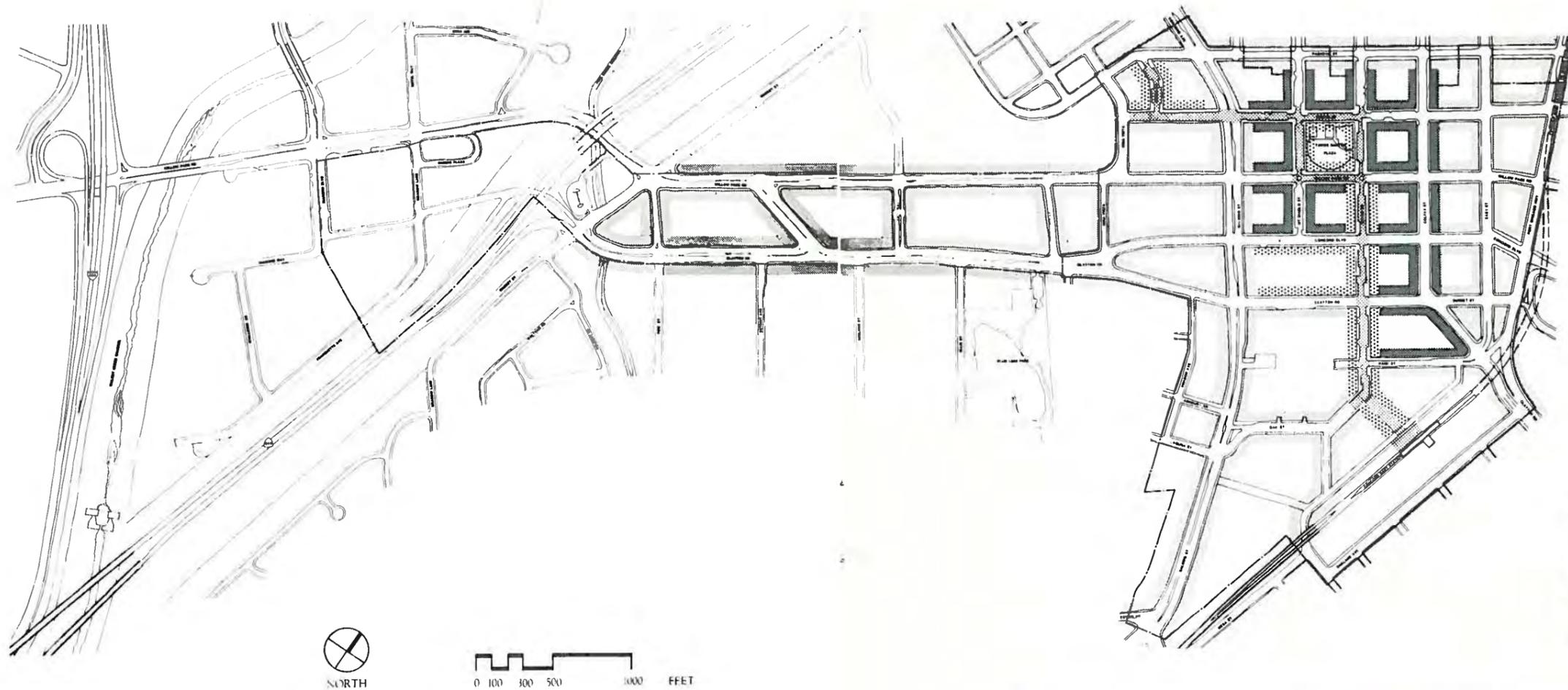


A view to the west down Salvio Street with Todos Santos Plaza in the middle-ground.

5. Suggestions are given for new public open spaces. The BART plaza, the "Adobe Paseo" at the Associates Bank and Chamber of Commerce blocks, and the retail courtyards adjacent to Todos Santos Plaza are a few of the more important spaces.
6. Streetlighting in the downtown is a system of compatible standards, fixtures, and lamps. Lighting for pedestrian ways, for streets and parking areas, and for buildings and landscaping are conceived as a unified design.
7. Finally, the Streetscape Concepts map illustrates a set of appropriate building footprints for downtown blocks. The footprints work with building setbacks and design guidelines to shape open spaces and to create the downtown streetscape.



A festival at Todos Santos Plaza as seen from Salvio and Grant Streets, showing the pedestrian emphasis of the streetscape.



STREETLIGHTS & PAVING

LEGEND

-  Low lights — 5 feet high
-  Tall light — 30 feet high
-  Low and tall lights
-  Special paving along major pedestrian way

DEVELOPMENT CONTROLS

The Development Controls map illustrates downtown zoning controls for building heights and setbacks. It uses symbols and annotations to explain concepts for the design of downtown development. Locations for major open spaces, arcades and colonnades, downtown gateways, and buildings are identified.

Development around Todos Santos Plaza is to be dense and urban, but relatively low in height at 55 feet maximum. The area north of the Concord BART Station is to have the tallest buildings in the downtown at a maximum of 200 feet. Between Todos Santos Plaza and the BART station, buildings are to be of medium height and density, with heights from 70 to 140 feet, in order to make a transition between the two different areas. A special height zone with a maximum height of 110 feet has been created along the west side of Galindo Street, south of Clayton Road, to ease the transition between the downtown and a residential area immediately to the west. The Clayton Road and Willow Pass Road corridor, between downtown and Freeway 242, is also to have buildings of medium height and density, with a maximum height of 140 feet, to serve as an urban pathway into downtown.



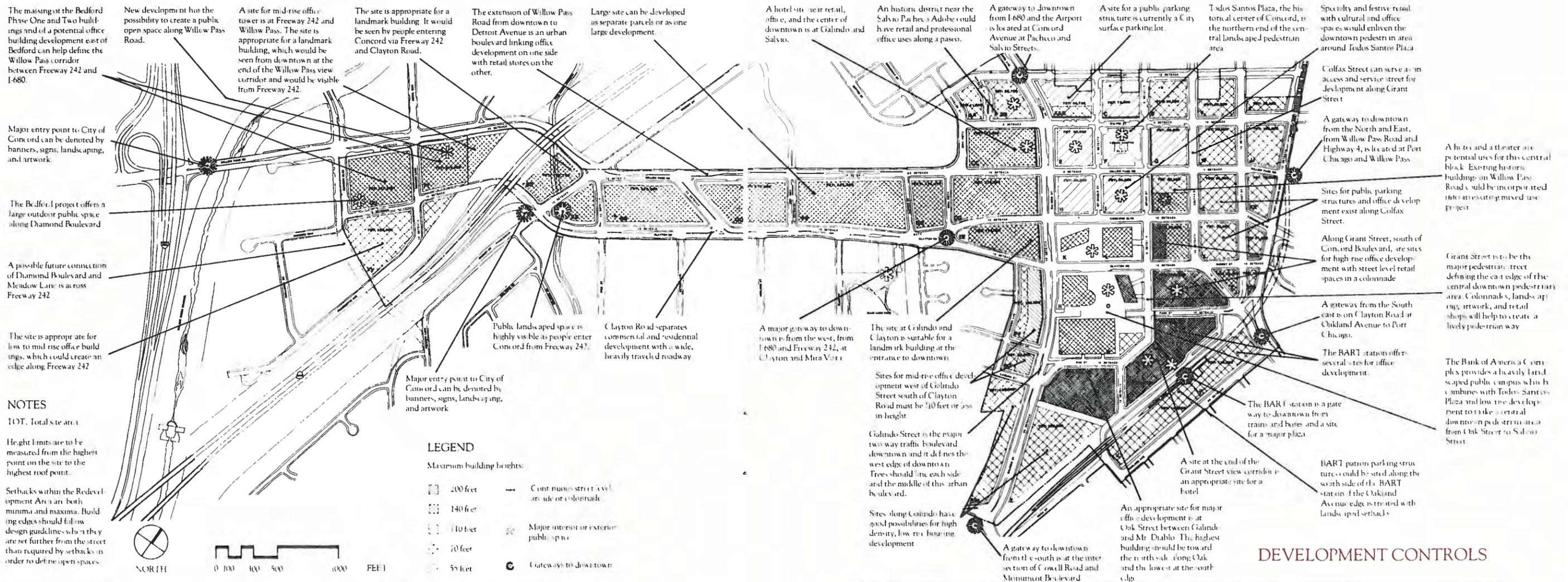
A view from the downtown along Clayton Road to the east, with Freeway 242 in the distance.

Setbacks are minimal in the core of the downtown, the area near Grant, Salvio, Galindo, and Oak Streets. The area bounded by these streets has low buildings and taller buildings with significant open space at the Bank of America campus to the south. The streets immediately surrounding this core have minimal setbacks, from zero to ten feet, to help create an urban environment.

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The urban design study model of downtown, looking south from Todos Santos Plaza to the BART station.



Galindo Street is the exception with a 20 foot setback, which is appropriate for the streetscape of a major boulevard through the center of downtown. As one moves further from the core of downtown, minor streets have 15 foot setbacks and major streets, such as Clayton Road, Willow Pass Road, and Concord Avenue, have setbacks from 15 to 25 feet.

Special features of the downtown and items worthy of note are indicated on the development controls map with symbols and written annotations. Arcades and colonnades are specified for building edges at grade around the core area, along Galindo, Salvio, Grant, and Oak Streets. Gateways to the downtown, major public open space locations, and descriptions of desired development concepts for important sites are included.

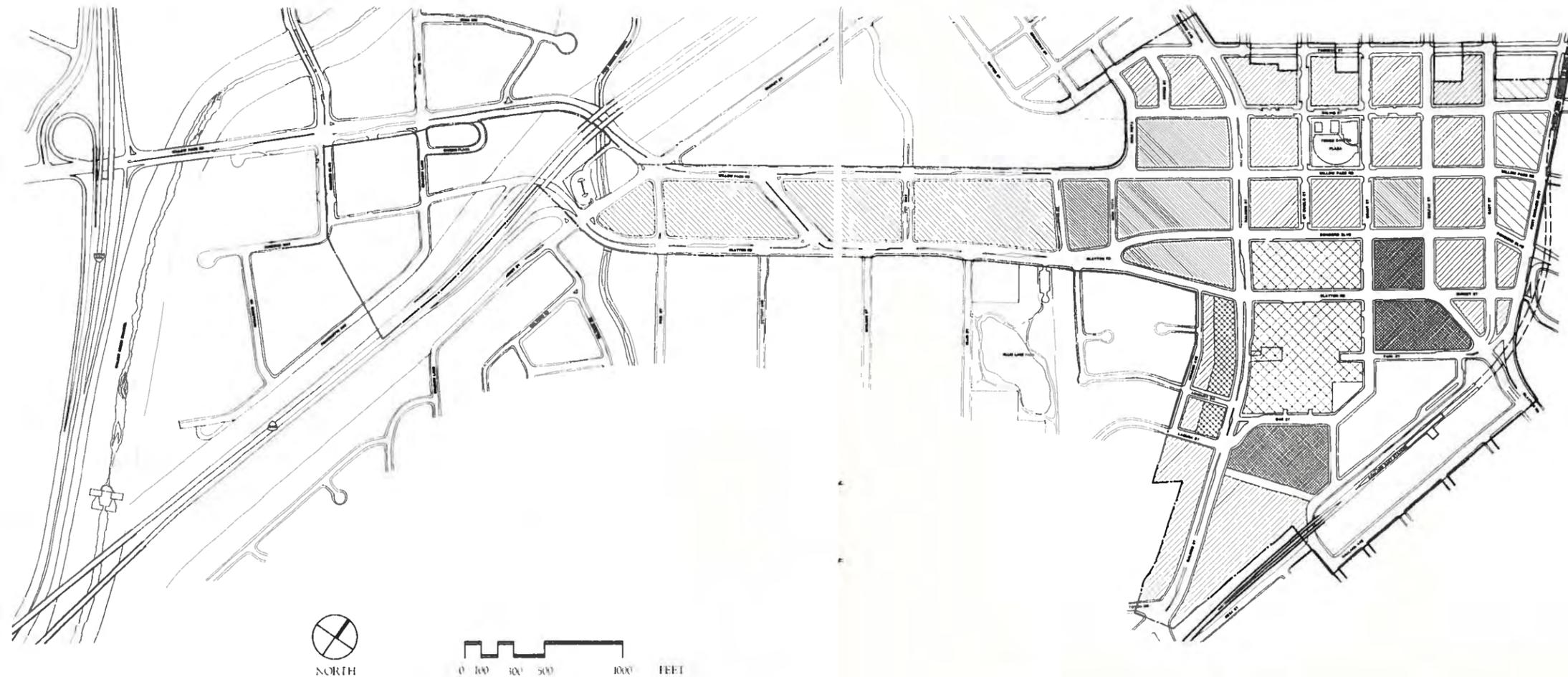


The renovated Perry House is now a restaurant and The Old Fire House and City Hall Building (beyond) is the Chamber of Commerce and Visitor's Bureau.

The maximum floor area ratios (FAR's) in the downtown, which are a measurement of the intensity of development, follow the heights indicated on the Development Controls map. The floor area ratio of a project is the gross building area of a development divided by the net site area, including parking levels at and above grade (see the Downtown Business District section of the City of Concord Zoning Ordinance). The highest FAR permitted is 5.0, for parcels near the BART station at the south end of Grant Street. The next highest FAR is 4.0, located mainly along Clayton Road from Freeway 242 to Galindo. An FAR of 3.0 is the maximum allowed on blocks around Todos Santos Plaza, and the Bank of America project is designated as 2.5. The lowest maximum FAR is 1.0, shown for blocks in the northeast part of downtown.



Downtown from the north, showing lower development near Todos Santos Plaza in the foreground and higher development near the BART station in the background.



FLOOR AREA RATIOS

LEGEND

The Floor Area Ratio (FAR) equals the maximum ratio of Gross Floor Area to Net Lot Area. Gross Floor Area includes parking levels at and above grade.

	1.0		3.0
	2.5		4.0
	3.0		5.0

TRANSPORTATION CONCEPTS

Downtown Concord needs a roadway system, parking, and a public transportation network that is continually improved to handle traffic effectively. A transportation plan and a construction program have been developed to respond to this need. Public funding sources for these improvements have been identified, but it will also take substantial funding commitments from the developers of the downtown properties to implement the program properly. Further information about the plan can be obtained from the City Department of Public Works.

The study area used in the transportation analysis includes all of downtown Concord and extends westerly to the Freeway 242 interchanges. A summary of the traffic volumes in downtown Concord follows:

Traffic Volumes--ADT (Average Daily Traffic)	1983	2000	Increase
Traffic generated in area:	87,000	181,000	94,000 (108%)
Through traffic:	88,000	107,000	19,000 (22%)
Total traffic:	175,000	288,000	113,000 (65%)

Downtown Concord is well served by public transit in comparison to most other suburban centers in the Bay Area. BART attracts a significant number of transit trips to destinations outside the study area, but does not serve a large number of trips with origins or destinations within the downtown study area. With the construction of new offices in downtown Concord, an increase in reverse commuting on BART can be expected.

Downtown Concord has good bus service operated by the Central Contra Costa Transit Authority. A major increase in bus usage to the downtown area will help reduce parking needs. The bus routes should avoid small downtown pedestrian streets and should follow the one-way street couplets and major boulevards as much as possible. Salvio, Grant, and Mt. Diablo Streets should be avoided, whereas Galindo-Concord Avenue, Willow Pass, Clayton Road, and the one-way couplets of Concord Clayton, Bonifacio-Pacheco, and East Port Chicago can be heavily routed.

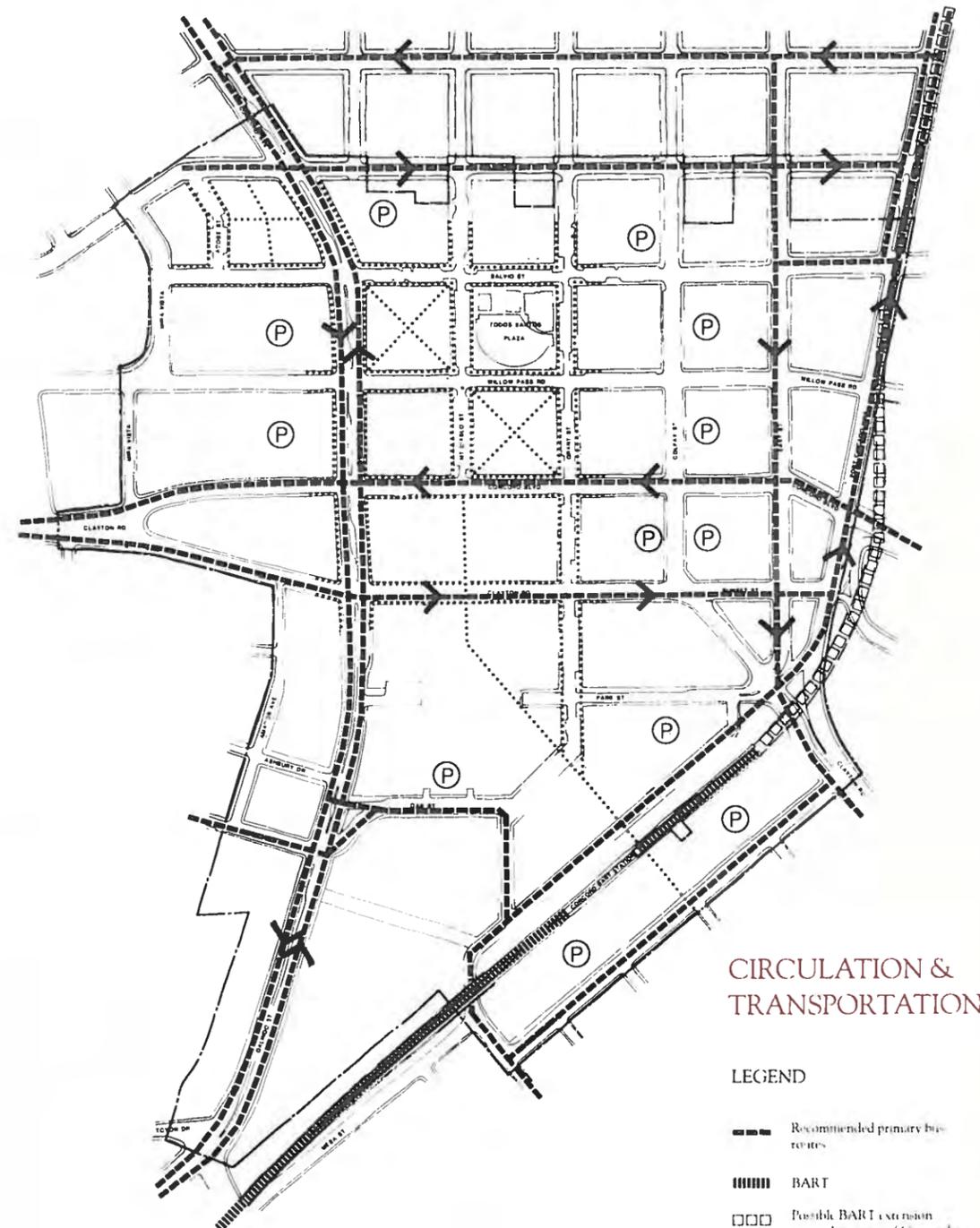
There appears to be good potential for reducing auto trips during peak hours by a transporta-

tion systems management (TSM) program of staggered work hours, carpools, vanpools, and priority parking policies. To this end the City has adopted a TSM ordinance (Ordinance No. 85-44), which requires the developers of projects of a certain size (e.g. 100 or more employee trips at peak hour) to submit a TSM Plan and execute a TSM Agreement with the City. The preliminary TSM Plan is submitted along with the developer's project application to the City Planning Department. The plan would include: the designation of a Transportation Systems Coordinator for the project, tenant/employee transportation surveys and vehicular trip forecasts, and strategies to reduce peak hour congestion. The TSM Agreement is signed and recorded after project approval and before occupancy. The agreement includes the terms between the City and the project sponsor for operating the project's TSM program.

Even with increased transit usage and a good TSM program, the expected increase in automobile traffic will require the City's proposed street improvements, which are shown on the street improvement map. In addition to these improvements, there will be minor modifications to some existing streets, such as removal of curbside parking, striping changes, and traffic signal improvements. City provided parking will also be needed near the core area of downtown to augment project specific parking. Potential sites north of Salvio, west of Galindo and east of Colfax have been identified as potential locations for city parking structures.

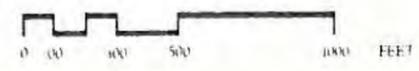


A train in the Concord BART station with recently completed tall buildings seen beyond.



CIRCULATION & TRANSPORTATION

- LEGEND
- Recommended primary bus routes
 - ==== BART
 - Possible BART extension to northern area of Concord
 - Major pedestrian circulation
 - (P) Potential sites for downtown public and private parking structures



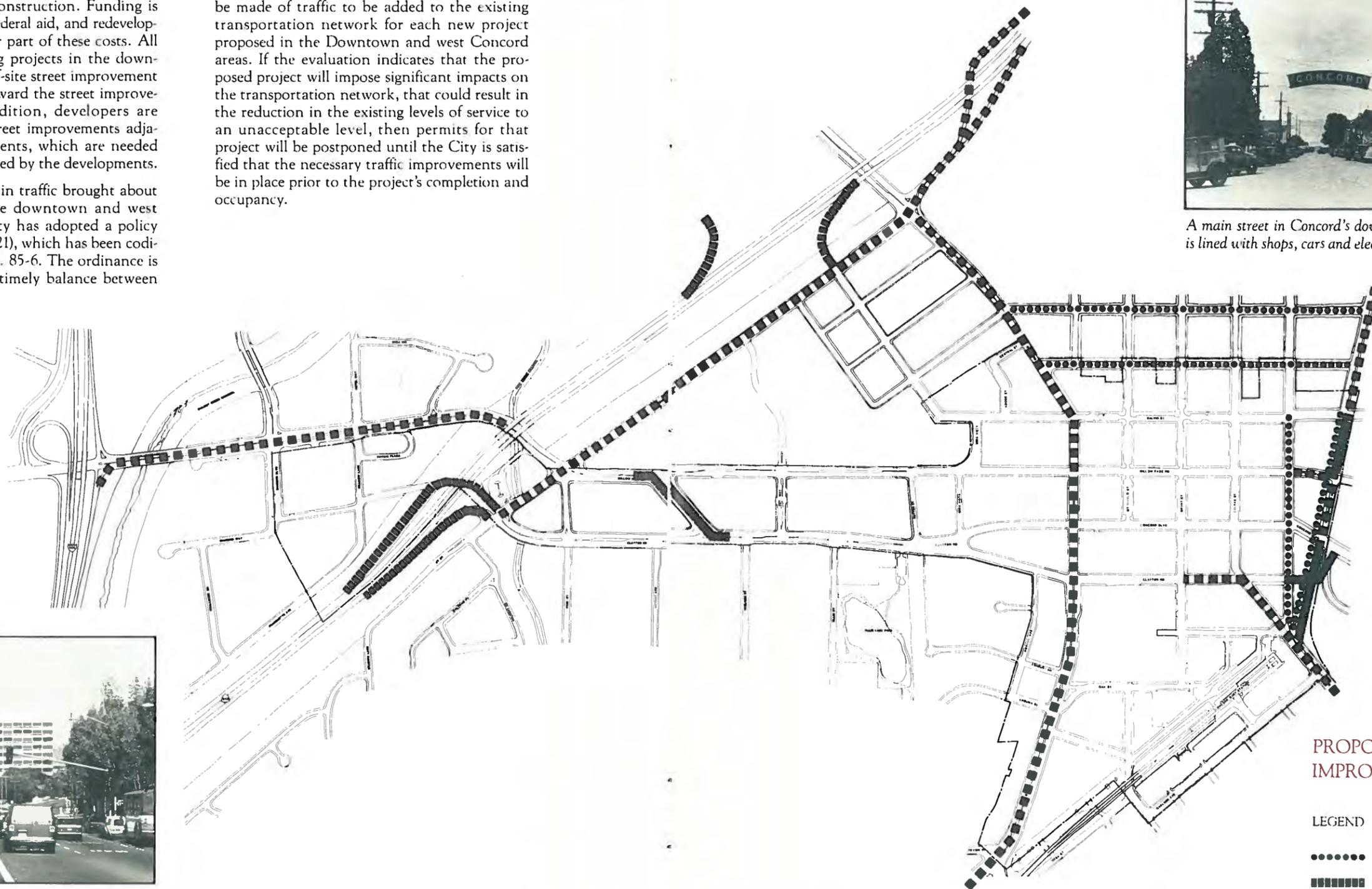
Studies of arterial streets and freeway access were made to determine approximate costs of land acquisition and construction. Funding is available from gas tax, federal aid, and redevelopment sources to pay for part of these costs. All developers constructing projects in the downtown are charged an off-site street improvement fee, which is applied toward the street improvement program. In addition, developers are required to build all street improvements adjacent to their developments, which are needed to handle traffic generated by the developments.

Because of the increase in traffic brought about by development in the downtown and west Concord areas, the City has adopted a policy (Policy and Procedure 121), which has been codified into Ordinance No. 85-6. The ordinance is intended to provide a timely balance between

land development and transportation improvements. The ordinance requires that an evaluation be made of traffic to be added to the existing transportation network for each new project proposed in the Downtown and west Concord areas. If the evaluation indicates that the proposed project will impose significant impacts on the transportation network, that could result in the reduction in the existing levels of service to an unacceptable level, then permits for that project will be postponed until the City is satisfied that the necessary traffic improvements will be in place prior to the project's completion and occupancy.



A main street in Concord's downtown of the 1940's is lined with shops, cars and electrical lines.



PROPOSED STREET IMPROVEMENTS

LEGEND

- New one-way street completions
- New roadways and ramps
- ■ ■ ■ Roadway and intersection widening



A view of downtown on Clayton Road, the major street to downtown from Freeway 242.



ART OPPORTUNITIES

An important part of this book is a public art program. Downtown Concord can be unique among cities in the region through a diligent effort to incorporate public art into the downtown. Artwork can be located outdoors along pedestrian ways, in public plazas, as well as in the courtyards, setbacks, and plazas of private developments. Indoors, art can be located in lobbies and in other major public building spaces.

Publicly sponsored outdoor art in downtown would be concentrated along Grant Street from the BART station to Todos Santos Plaza and from the plaza west along Salvio Street to Adobe Street. Within this area elements of the streetscape, such as paving, tree grates, fixtures, furniture, lighting, and equipment could be subjects for art competitions. Individual or repetitive items designed through the art program should be compatible with the palette of current streetscape elements around Todos Santos Plaza, which are to be extended throughout the downtown. In addition to the design of parts of the streetscape, specific works of art (e.g., sculpture, murals, and constructions) could be purchased or commissioned for visually prominent locations.

Major gateways to the downtown are important locations for publicly sponsored art. In some or all of these locations, art in the form of constructions, sculpture, lighting, or graphics could be used in conjunction with landscaping to create memorable entrances to the downtown.

The Concord City Council has envisioned that public and private buildings, constructed in the downtown, would have interior and exterior public art as part of the development effort. Artwork could be placed in lobbies and other public indoor spaces, as well as in exterior spaces such as gardens and plazas. The subjects of such art may include traditional sculpture or painting and may also extend to paving patterns and materials, wall murals, waterworks, hangings, and other constructions.

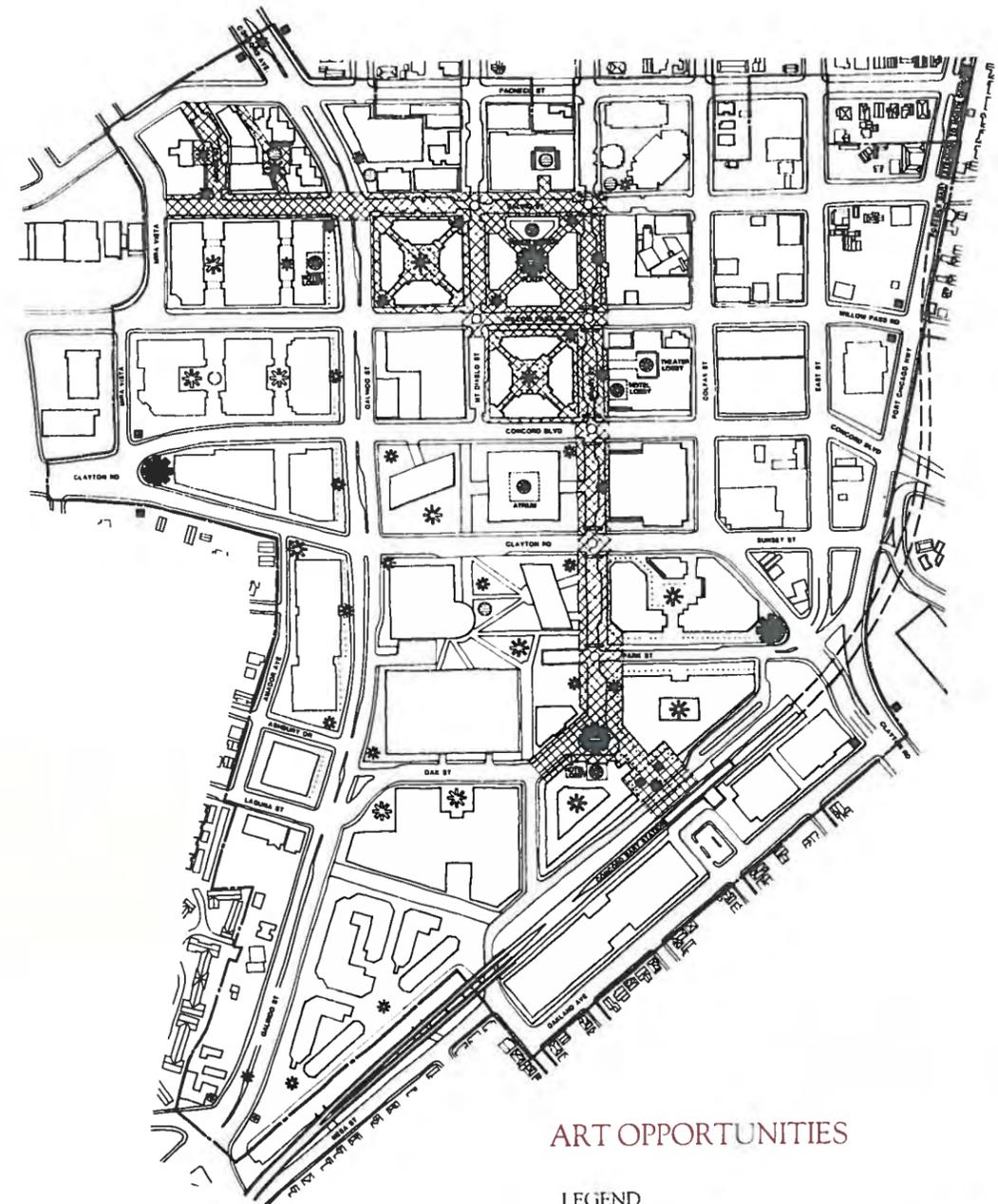
The City of Concord has adopted an ordinance which levies "one-half of one percent for art in public places" (Ordinance No. 85-13). The ordinance enables the City to collect one-half of one percent of the construction budget of each development in the downtown for public art. The Redevelopment Agency has also pledged one percent of its yearly tax increment revenue as well as one-half of one percent of its tax allocation bond proceeds for art in public places

within the Redevelopment Project Area (Resolution No. 85-294). The Concord Arts Committee will advise the City's Visual Arts Coordinator on the implementation of the Art in Public Places program. The final approval of all art in public places programs will be given by the City Council, upon receiving recommendations from the Concord Arts Committee.

Publicly sponsored art for this program can be acquired through direct purchase, direct commission, or as the result of design competitions. It is hoped that this major program of public art will generate great excitement and civic interest in the downtown and provide a continuing forum for discussion of the downtown environment.



A tile rosette set into the street at the intersection of Willow Pass Road and Mount Diablo Street.



ART OPPORTUNITIES

LEGEND

- Major art work (public)
- * Significant art work (public)
- Significant art work (private)
- ⊙ Interior art work (private)
- ⊕ Water feature
- ⊗ Major art work with water feature
- Gateways to Downtown
- ⊘ Arterials (on Grant Street)
- Vertical surface art (exterior)
- Vertical surface art (interior)
- ⊞ Major pedestrian areas



BUILDING PRESERVATION

Concord has a commitment to preserve the important heritage of its downtown built environment. The map designates buildings and areas to be enhanced and preserved. The buildings shown on the map are both new and old, some of which have historic and architectural significance. Three categories of buildings are designated: newer buildings, older buildings, and buildings that are relatively old and small, which could possibly be moved.

The newer downtown buildings are mostly large projects built in the last two decades, which will exist for at least several generations. The buildings include: (1) Heritage, (2) Plaza Tower, (3) City Parking Garage at Salvio and Colfax Streets, (4) Salvio Pacheco Square, (5) Park Terrace Apartments, (6) Bank of America, Concord Main Office, (7) Bank of America, Concord Center complex, (8) One Concord Center, phase one and in the future, phase two, (9) Pacific Telephone Switching Building, (10) Professional Office Building at Salvio and East Streets, (11) The Presbyterian Church buildings at Salvio and Colfax Streets, and (12) The Concord BART Station.

Some of the older buildings have historic or architectural significance and some do not. The buildings are listed with their historic name first, where appropriate, and their current use in parentheses: (1) The Foscett Building (TR's Restaurant), (2) The Old Fire House and City Hall Building (Chamber of Commerce), (3) The Perry House (restaurant building at Galindo and Salvio Streets), (4) The Salvio Pacheco Adobe (Associates National Bank), (5) The Fire House at Willow Pass Road and Grant Street, (7) The Veterans' Hall, (8) The Mackenzie-Collins House at Salvio between East and Port Chicago, and (9) The Keller House on Clayton Road near Sutter Street.

The third category of buildings shown on the map are relatively old and small buildings, which may have historic or architectural significance and which could possibly be moved into historic building areas. The category includes: (1) The Ivy House and (2) The Masonic Temple.

Two potential historic building areas have been identified within the Central Concord Redevelopment Project Area. Other potential areas also exist, which are near downtown and outside the Redevelopment Area. The two areas within the Redevelopment Area, which are indicated on

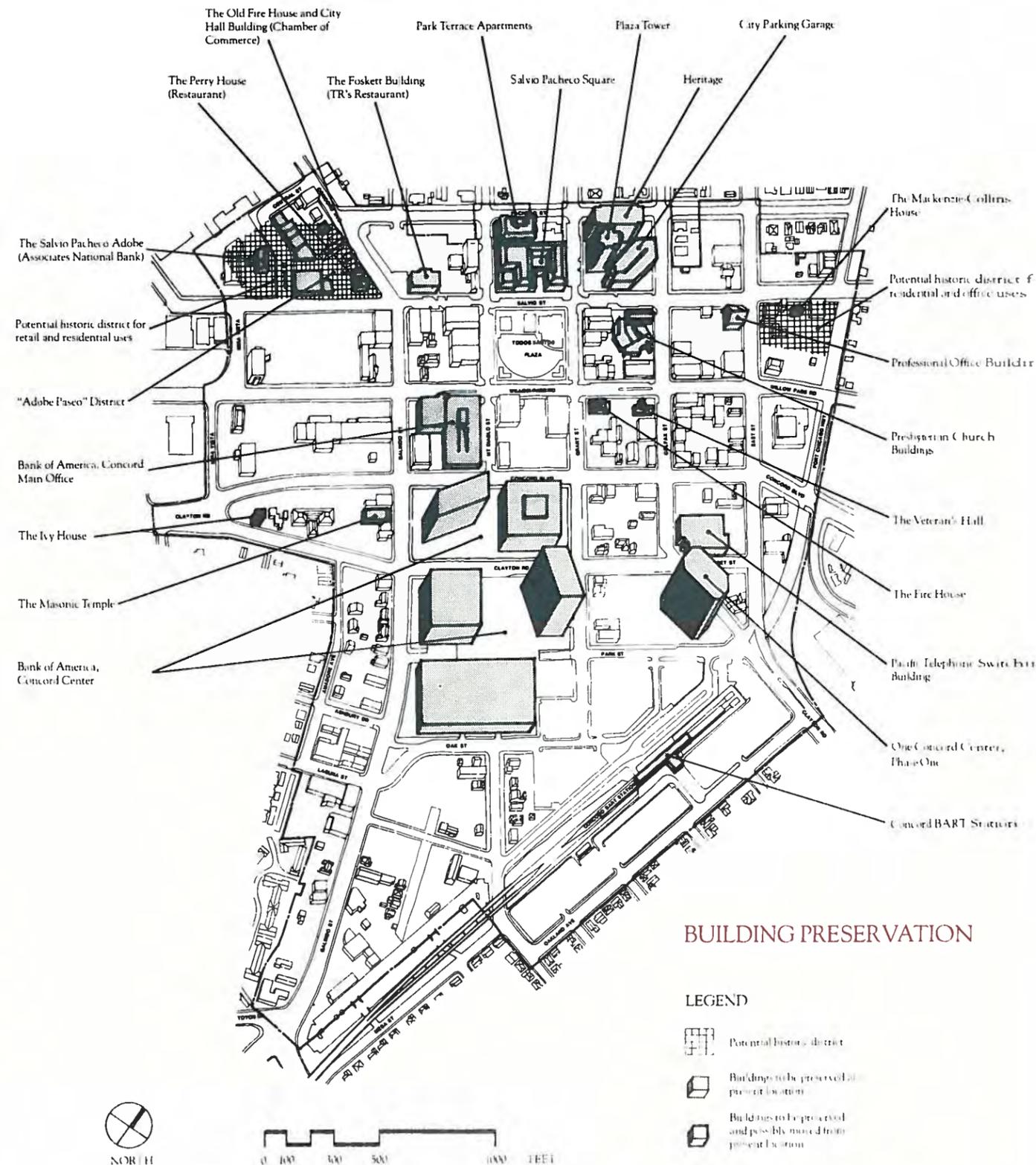
the map are: (1) The "Adobe Paseo" area, bounded by Concord Avenue, Salvio, Adobe, and Central Streets, which is suitable for retail and office uses, and (2) a half-block area on the south side of Salvio Street, bounded by East Street and Port Chicago Highway, which is suitable for residential and office uses.



The Foscett Building at Salvio and Galindo Streets before renovation.



The Foscett Building after renovation with new streetlighting, streetscape elements, and a fountain.



BUILDING PRESERVATION

LEGEND

- Potential historic district
- Buildings to be preserved at present location
- Buildings to be preserved and possibly moved from present location

URBAN DESIGN GUIDELINES

The Urban Design Guidelines set forth standards to be used in the creation and public review of building and open space designs. The guidelines apply to private developers and their architects as well as to public sponsors who propose designs for the downtown. The guidelines are the most detailed design policy instruments that a community can create short of creating designs. The Design Guidelines sometimes give quantifiable standards, but more often they are qualitative and require judgement in their application.

The guidelines are a combination of recommendations specific to Concord and recommendations which are common to the design of urban areas throughout the western world. Guidelines such as “Todos Santos Plaza” and “Streetlighting” refer specifically to downtown Concord, whereas “City Outdoor Rooms” and “Bases of Tall Buildings” are applicable to many urban environments.

The Design Guidelines are a manifestation of the community’s intentions for its urban environment. They are a tool to evaluate development proposals submitted to the City. They are to be used by Concord public bodies: the Redevelopment Agency, the Planning Commission, and the Design Review Board.

The Design Guidelines should not be a static body of standards, but rather should evolve over time as experience validates some guidelines over others, and as new situations require new guidelines. The body of guidelines need not necessarily be applied as a whole to all projects; particular guidelines and groups of guidelines will be relevant to particular projects or sites.

The use of Design Guidelines is not a substitute for design talent or good intentions on the part of a sponsor. In addition to the guidelines, the City must also communicate a desire to have the highest quality of design from professionals proposing environmental change in the downtown.



The urban design study model of downtown looking to the northeast.

Each guideline has a statement followed by a discussion of the guideline's purpose, nature, and application. Many guidelines use diagrams and photographs to illustrate design ideas. The titles of the guidelines, as they appear in pairs, are:

HISTORIC ELEMENTS
BUILDING CONTEXT

DEFINITION OF PLAZAS
TODOS SANTOS PLAZA

BUILDING SETBACKS
CITY OUTDOOR ROOMS

BULK OF TALL BUILDINGS
BASES OF TALL BUILDINGS

BUILDING MATERIALS
COLOR OF TALL BUILDINGS

STREET LEVEL USES
SIDEWALK LANDSCAPE

SIDEWALK HARDSCAPE
BUILDING SIGNAGE

ARCADES AND COLONNADES
SIDEWALK CANOPIES

STREETLIGHTING
BUILDING LIGHTING

WATER FEATURES
ART IN PUBLIC SPACES

SITE UTILITIES
PEDESTRIAN CIRCULATION

PARKING GARAGE HEIGHT
PARKING GARAGE TREATMENT

PARKING GARAGE ROOFS
BUILDING ROOFS

HISTORIC ELEMENTS

New building projects shall respect the architectural heritage of existing buildings in scale and form.

Discussion:

Downtown Concord's architectural history dates back to the early days of California. Older buildings display design concepts of the Spanish Colonial and Victorian eras of California history. This heritage should be respected by new development in both scale and form. While the City does not intend new buildings to be imitations or copies of period architecture, the generic principles of early California architecture can foster design continuity in the downtown.

These generic principles are based on ideas that are intrinsic to California's climate and lifestyles. These principles, manifested in historic architectural expressions, are still valued today as examples of architectural quality. Many guidelines in this book deal with such principles. For example, arcades and colonnades are specified for parts of the downtown, fountains and water features are encouraged, and the use of building materials with "substance" is discussed.

This guideline is intended to help developers and designers understand the City's concern about historic continuity. For a more detailed discussion of Concord's architectural heritage, see the report entitled: "Todos Santos Design Vocabulary", published in 1979 by the Concord City Planning Department and the Concord Redevelopment Agency.



The moving of the Keller House before renovation.



The Keller House after renovation at its new site on Clayton Road.

BUILDING CONTEXT

New buildings shall be designed to be compatible with existing adjacent buildings.

Discussion:

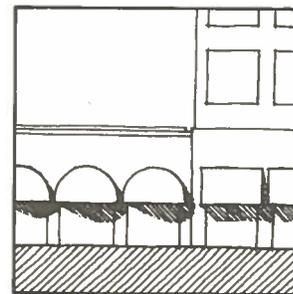
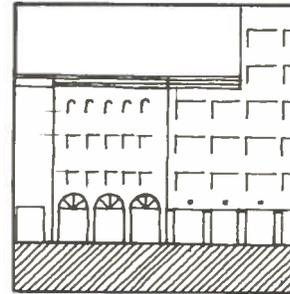
The existing buildings of a city provide references for the design of new ones. To the extent that the height, scale, and texture of the new buildings respond to what is already there, the city is continuously knitted together. Conversely, disregard of the existing building pattern tends to fragment the city and reduce its coherence.

Architectural devices can be used to provide transitions between the old and the new. Such devices include aligning cornice lines, continuing a pattern of wall openings, using similar materials, and relating overall building proportions.

The scale of modern buildings can often be very different from older, existing ones. Therefore, this guideline cannot be applied arbitrarily or dogmatically, but must be used with sensitivity and discretion.



New and old buildings coexist around Todos Santos Plaza.



DEFINITION OF PLAZAS

Plazas and courtyards shall be defined on at least three sides with buildings, walls, or landscaping.

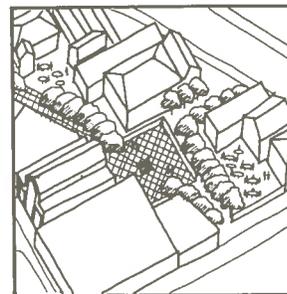
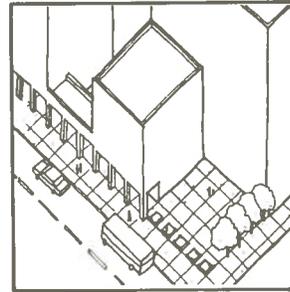
Discussion:

Open spaces, plazas, and courtyards in cities are defined by their edges. If the edges are weak, the spaces seem amorphous and lack focus. The most memorable and successful open spaces are well defined by building facades, arcades, garden walls, bosks of trees, or other elements having strong character and clear geometry. Examples of historically successful spaces of this type are Piazza San Marco in Venice, Rockefeller Plaza in New York, and Union Square in San Francisco. Concord has fine examples in Todos Santos Plaza and the courtyard of Salvio Pacheco Square. It is difficult to find successful urban spaces that lack this definition. In fact, without such definition we cannot recognize that the space exists.

Definition and enrichment within open spaces can be achieved by the use of landscape, hardscape, and water features. Fountains, pools, and defined waterways can be combined with planting and paved areas to visually and functionally enliven urban open spaces. Attention should be given to the patterns of sunlight and shadow in open spaces, created by the elements defining each space.



A small plaza is defined within Salvio Pacheco Square.



TODOS SANTOS PLAZA

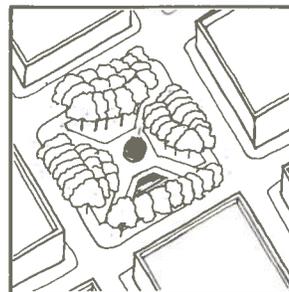
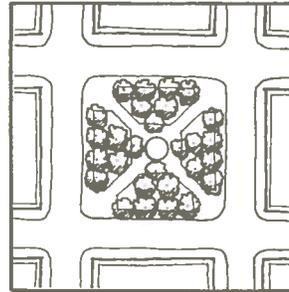
Buildings facing Todos Santos Plaza shall be perceived to create continuous facades along the four frontages and at the four corners which bound the plaza. The buildings shall have cornices which have a minimum number of height changes, which are approximately the same height as Salvio Pacheco Square, and which in no case are less than 30 feet in height at the street edges. Facades shall have breaks only for defined plazas at mid-block and only for 25% or less of a block's length.

Discussion:

Todos Santos Plaza is the historic core of downtown Concord. As such, rigorous attention should be given to guiding the form of development around it. Facades, cornice lines and heights of buildings around the plaza need to be carefully controlled in order to define the plaza clearly.



The facade of Salvio Pacheco Square defines the north side of Todos Santos Plaza.



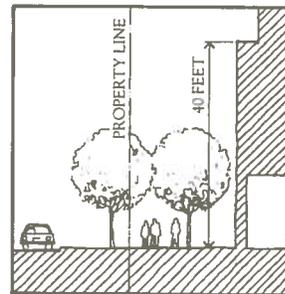
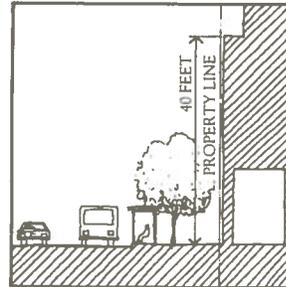
BUILDING SETBACKS

Required building setbacks from property lines, as set forth in the Zoning Ordinance for the Downtown Business District, shall be considered maxima and minima. On streets with required setbacks of zero or ten feet, street facades shall be at the setback lines for at least the first 40 feet of their height and for at least 75% of their length.

Discussion:

The intention of this guideline is to give definition to street spaces. The guideline provides for a relatively constant 40 foot height at setback lines, where buildings are at least 40 feet tall. Above that height, buildings may rise vertically, step back, or otherwise vary in shape and distance from the setbacks. For each block frontage the building facades will be at the setback line for 75% or more of their length. The establishment of cornice heights and limits on breaks in facades serve to integrate the architecture of downtown and to carefully shape public outdoor spaces.

For building facades on streets with zero setbacks, retail shops with or without arcades and colonnades are intended to occur at street level. It is paramount for the vitality of downtown streets that blank walls not be at the pedestrian level along streets with zero setbacks. Where setbacks are ten feet or more, landscaping can be combined with facade treatments to create a favorable pedestrian environment, especially in areas where retail uses are inappropriate.



CITY OUTDOOR ROOMS

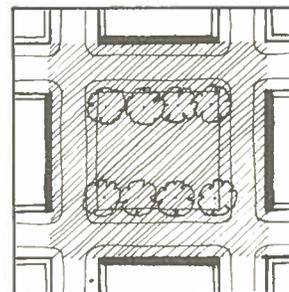
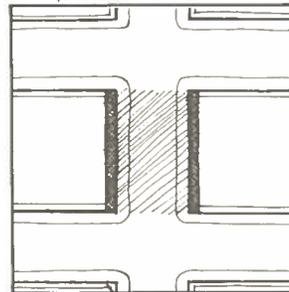
Since streets and plazas are a city's outdoor rooms, building facades shall perform a civic role as the walls of the rooms.

Discussion:

Facades are part of the public realm of the downtown and, in fact, are the walls of public open spaces. Facades need to be compatible with nearby buildings and to reflect the nature, size, and character of outdoor rooms, which vary from narrow streets to large urban open spaces. While it is not possible to specify what this role may require of a particular building facade in a particular place, this guideline is intended to remind designers that the exteriors of their buildings have a larger role than just reflecting interior functions. This guideline challenges architects to step outside their particular project and consider building design within the larger urban design context.



Facades of 1908 buildings clearly define the public space of Main Street, Concord.



BULK OF TALL BUILDINGS

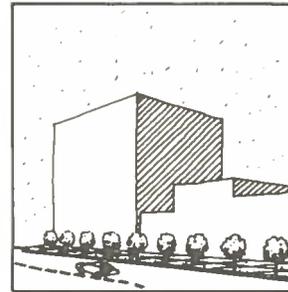
The bulk of tall buildings shall be minimized by articulating the mass with changes of plane, stepped terraces, and other architectural means.

Discussion:

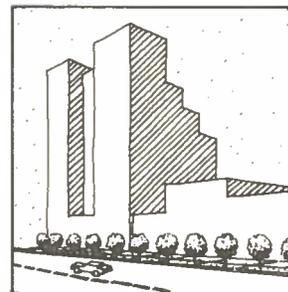
Tall buildings can appear to loom over the city due to their bulk, particularly the recent generation of office buildings with large floor plates. Bulky buildings can block views to hills around the city and block sunlight to plazas within the downtown. These effects can be mitigated, or eliminated, by design strategies that break up the mass by articulation of wall planes. The tops of these buildings, including the roofline and the facades of the top floors, should also be given design consideration because they become prominent visual features of the City's skyline.



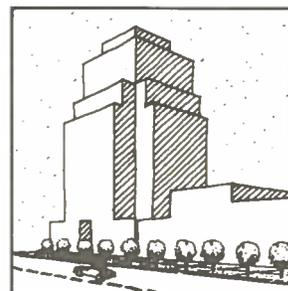
Stepped-backs and material changes minimize bulk in a new office tower on Clayton Road.



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DESIRABLE



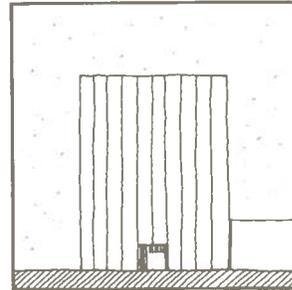
DESIRABLE

BASES OF BUILDINGS

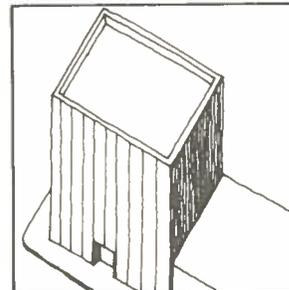
The bases of tall buildings shall be designed to relate to the streetscape and the environment of pedestrians.

Discussion:

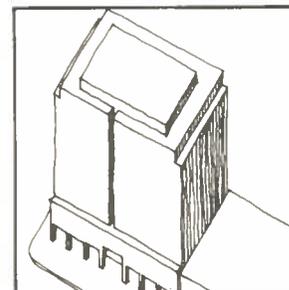
The bases of tall buildings, usually the first two or three floors, are experienced by pedestrians at close distance. The base of a building belongs to the streetscape, not to the skyline. The lower floors should reflect these conditions in design solutions which emphasize color, texture, and other treatments of visual interest at the pedestrian scale. This is the place for colonnades, awnings, rich materials, signs, special amenities, and small scale design elements. This is not the place for the more generalized curtain wall patterns that are often used on the upper stories of tall buildings.



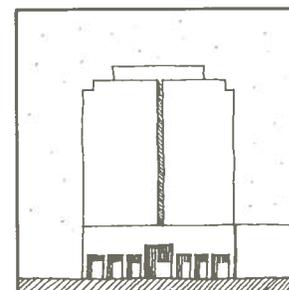
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DESIRABLE



DESIRABLE

BUILDING MATERIALS

Materials that are permanent, durable, and compatible with the architectural character and history of Concord shall be used on building facades in downtown.

Discussion:

Buildings in downtown should have substance and durability in reality as well as in appearance. Materials like brick, stone, tile, stucco, and some forms of concrete have connotations of permanence and substance that are appreciated by urban citizens. These materials are also part of the vernacular of early California architecture, which comprises much of downtown Concord's heritage. Conversely, many metal and glass curtain walls have a surface reflectivity and undulation which to many people denote a flimsy or temporary quality.

Glass should be used carefully with other exterior materials. Special attention should be paid to glass color and reflectivity, surface curvature, opening sizes, color and material of frames, and changes of plane between glass and other exterior materials. Walls which are glazed for more than fifty percent of their surface area are discouraged. Glazing should not have reflective coatings applied to exterior glazing surfaces.



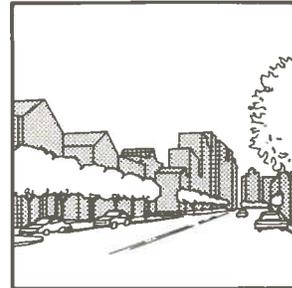
Some of the recent tall buildings have non-reflective and light colored materials.

COLOR OF TALL BUILDINGS

Light to medium values of color shall be used on the exterior of tall buildings, especially above the third floor.

Discussion:

Dark buildings appear ominous on the skyline and may seem more massive than they actually are. Dark colors absorb light and increase heat gain. Conversely, lighter shades of color on tall structures can visually reduce their mass, make the downtown appear brighter and more cheerful, and help reduce cooling loads.



UNDESIRABLE



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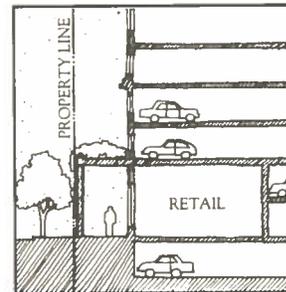
S TREET LEVEL USES

Retail stores, restaurants, services, and other high intensity pedestrian uses shall be placed on the ground levels of all buildings fronting major pedestrian streets.

Discussion:

The ground floors of buildings should have the most active uses. The ground level is where people walk and drive, and where people expect to find the goods and services they need. If street frontages are active, then the city is vibrant. Conversely, blank facades, open parking garages, and non-active uses at grade level produce dull and dangerous places.

Not every street can be lined with shops, but streets like Grant, Salvio, Mt. Diablo, and Willow Pass should have a preponderance of retail activity. Secondary and minor pedestrian streets might have retail activities only at the corners or occasionally at important places in the middle of the block. Certain kinds of office space, such as insurance brokers or travel agents, are also appropriate in locations where retail stores may not be economically possible.

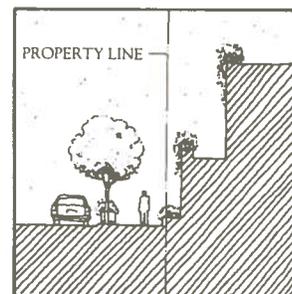
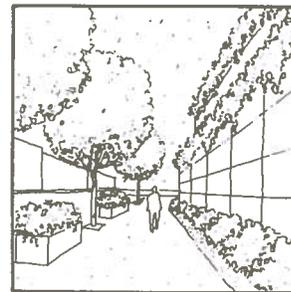
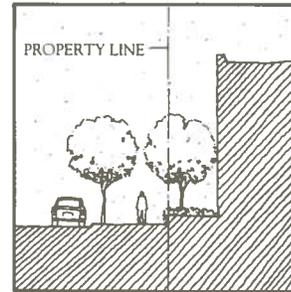


SIDEWALK LANDSCAPE

On streets where ground floor retail or office uses are not economically possible or appropriate, pedestrian ways shall be enhanced with planting, art, and special architectural treatments.

Discussion:

Major arterials and minor streets may have significant pedestrian traffic, even though few shops or restaurants may be located along them. Pedestrian comfort and civic amenity should therefore be a prime design consideration. Blank walls without special treatment at street level along a pedestrian way should be avoided; amenities, including landscaping, art, and architectural treatments, can occur in various ways where blank walls occur. Landscaping can include ground covers, climbing plants, shrubs, and trees.



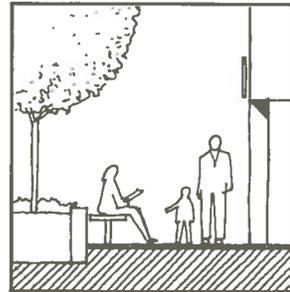
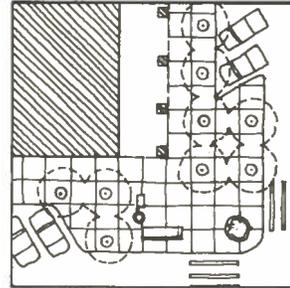
S IDE WALK HAR DSCAPE

Sidewalk hardscape elements of the Todos Santos Plaza area, such as pavers, planters, bollards, and benches, shall be used for public streetscape in the core area of downtown. Private projects adjoining public streets and plazas shall use hardscape elements which are compatible with public streetscape elements.

Discussion:

Streetscape design is critically important to the quality of downtown. This guideline is meant to promote design continuity in the pedestrian hardscape of the downtown. Building heights and styles may vary within downtown, but the consistent use of hardscape elements can supply part of the “glue” that holds a district together.

Street hardscape should be used with other streetscape elements, such as colonnades and landscaping, to unify the BART Station and Todos Santos Plaza areas into one downtown. Elements currently in the design vocabulary of the hardscape around Todos Santos Plaza could be expanded to include such items as shelters, trash receptacles, newspaper racks, newsstands, and information kiosks.



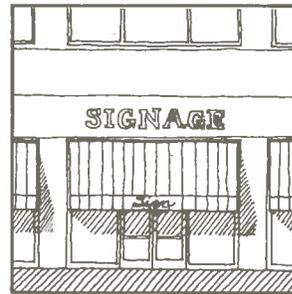
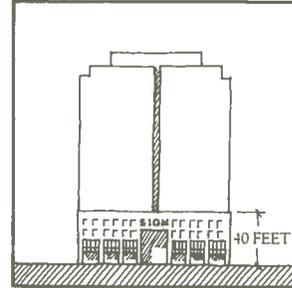
BUILDING SIGNAGE

The location of signs on buildings shall be limited to the first 40 feet in height above street level, except for a maximum of two building identification signs at the parapet level of taller buildings. Building identification signage shall be integrated with the design of a building's base and top. Retail signage shall adhere to a design program for the building's storefronts.

Discussion:

Signs located above the first few levels of a building facade are often an advertisement for the distant viewer rather than an informative and vital part of a cityscape. In order to be seen by the distant viewer, the size, color, and lighting of building signs are often out of character with the building facades and are detrimental to the City's skyline. Therefore, signs at the top of buildings for identification should be carefully integrated with the scale, color, and articulation of the building design.

Signs can add vitality to a streetscape or plaza when incorporated in building facades at the first few levels above the street. Retail shops, walk-in offices, public entrances, and building names can provide an exciting mix of signs to be seen by pedestrians and vehicular passers-by. However, it is important that design of signage be controlled at a project scale, so that it is integrated with specific building design as well as streetscape design. To achieve design control of retail signage at building storefronts, developers and their architects should require specific designs from individual tenants and a system of signage rules and criteria for inclusion in tenant leases.



A RCADES AND COLONNADES

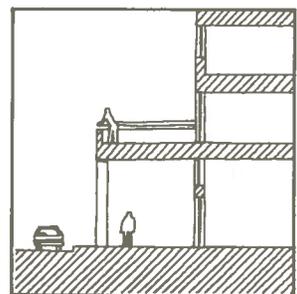
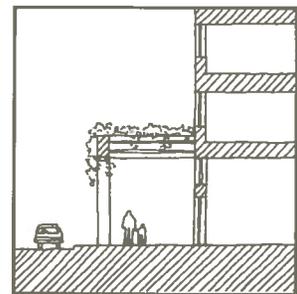
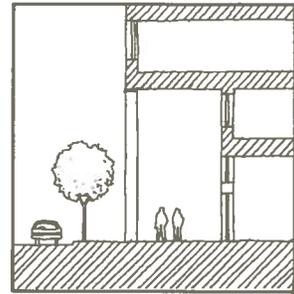
Pedestrian areas and sidewalks in the downtown shall have arcades and colonnades wherever possible. The map in the “Development Controls” section indicates recommended locations of continuous street level colonnades around the core pedestrian area of downtown.

Discussion:

The quality of a city’s design is determined to a great extent by how it is experienced by pedestrians. In Concord, where the climate is warm and sunny, pedestrian comfort can be greatly increased by using architectural devices such as covered arcades and colonnades at the first story of buildings. In addition to weather protection, these devices can bring appropriate scale to shopping and business activities, highlight retail goods and services, and create an environment of comfort, enjoyment, and civic pride.



An arcade offers relief to pedestrians from heat, rain, and traffic.



SIDEWALK CANOPIES

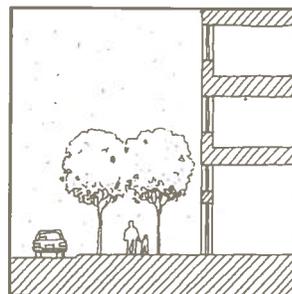
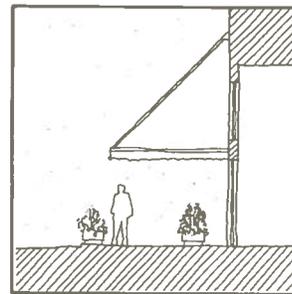
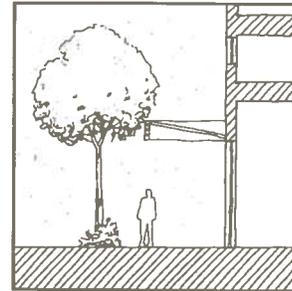
In locations where building arcades and colonnades are inappropriate because of the nature of the street or the building uses, pedestrian areas and sidewalks shall be enlivened with awnings, trellises, landscaping, and other means to create full or partial canopies over pedestrian ways.

Discussion:

Not every street has the activity or importance to justify major architectural gestures such as arcades or colonnades. However, even minor building frontages should address the sidewalks with the pedestrian and streetscape in mind. Awnings over entrances, decorative features on walls, landscaped planters, trees, and similar treatments can make the streetlife and appearance of the City attractive and vital. Fabric awnings and glazed canopies are encouraged and metal awnings are discouraged.



A planted trellis over the sidewalk provides a protective canopy for pedestrians.



S T R E E T L I G H T I N G

The two types of streetlights currently in use around Todos Santos Plaza shall be used throughout the larger downtown area, including the BART Station area and Galindo Street.

Discussion:

All public street and pedestrian lighting in the downtown should be compatible in style, fixture color, and lamp color. The light fixtures will help to give the downtown streetscape a unified appearance during the day and distinctive lighting at night. The lights recently installed around Todos Santos Plaza have dark blue standards and sodium vapor lamps. Public fixtures installed in other sections of the downtown should be identical to or compatible with these fixtures.

The two types of fixtures currently used at Todos Santos Plaza are a low, seven to ten foot high, pedestrian standard and a tall, twenty to thirty foot high, street and surface parking area standard. Other types of public and private fixtures, which could be added to the two basic streetlight fixtures, are lights to highlight landscaping, fountains, and works of art.



High and low streetlights are part of the streetscape design of north Grant Street.



BUILDING LIGHTING

Within building projects, exterior lighting for pedestrian areas, building facades, parking garages, landscape elements, and design features shall be complementary in style, color, and lamping with public street and pedestrian lighting.

Discussion:

The purpose of this guideline is to encourage an abundance of high quality outdoor lighting to give the downtown vitality and sparkle at night. Each project must share in this enterprise. It is not necessary to duplicate light fixtures used by the City, but fixtures proposed for building exteriors must work effectively with streetlighting.

Building lighting includes storefront display lights, public lobby lighting, illumination of signs, and special feature lighting such as high-lighting facades and tops of buildings. Street level lighting of a building can augment City streetlights in many ways, for example, by accenting a sidewalk, a colonnade, or the vault of an arcade.

Parking garage lighting must be shielded from public view as much as possible, especially from street level vantage points. Placing fixtures back from the street edge and behind spandrels and beams can be combined with a dark ceiling color to minimize the views of lights on intermediate levels. For the roof of a garage, light standards can be kept as short as possible and supplementary lighting can be added to the interior surfaces of parapet walls. In all cases, specified lenses, reflectors, and shields of light fixtures should reduce direct views of garage lighting.



Lighting on the building wall will illuminate the arcade interior as well as the sidewalk.

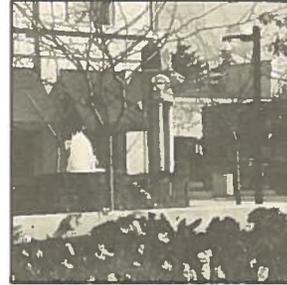


WATER FEATURES

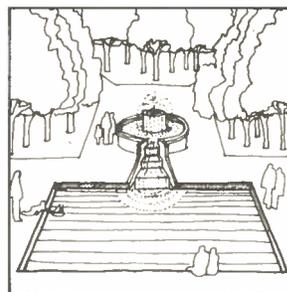
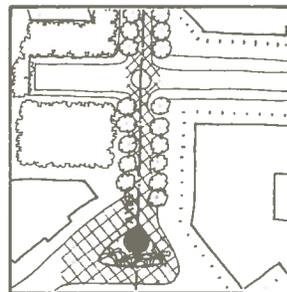
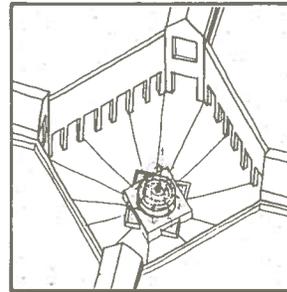
Water features such as pools, sprays, fountains, and sculptures shall be provided in outdoor public spaces, as appropriate. Water features shall not be isolated elements in the landscape, but shall be integrated functionally and visually with the overall design of plazas and courtyards.

Discussion:

Water seems to be a universal “good” in outdoor spaces. People respond positively to pools, fountains, and other water features. Water features are also a traditional element in early California architecture. In addition, running water provides acoustical masking of traffic noise and evaporative cooling in hot weather.



The fountain at Galindo and Salvia enlivens a pedestrian corner.

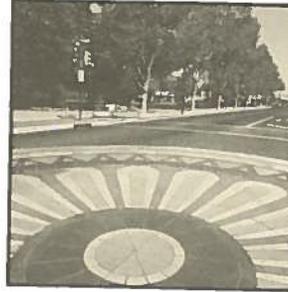


A RT IN PUBLIC SPACES

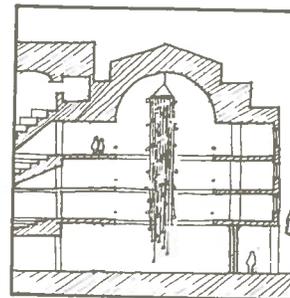
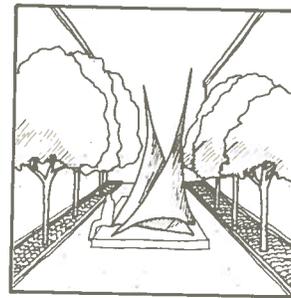
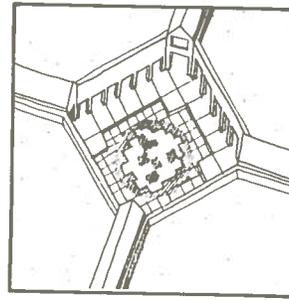
Works of art shall be included in the development of major outdoor and indoor public spaces.

Discussion:

It is the City's intention that public art become a main feature of downtown Concord. Murals, hangings, mosaics, sculpture, flags, banners, light and water events increase the usage and enjoyment of downtown public spaces. Programs have been established which will provide works of art within public spaces of both privately and publicly funded building projects. The purpose of this guideline is to alert building sponsors to the City's desire that art be a part of at least the public portions of their projects.



A work of art in the street at Todos Santos Plaza.

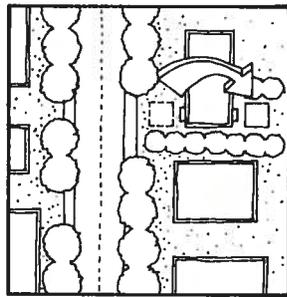
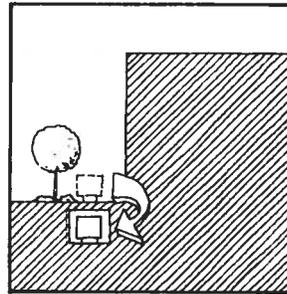
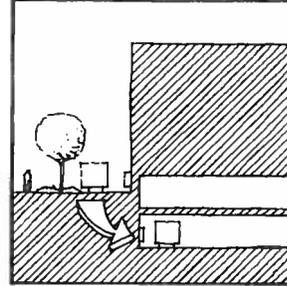


S I T E U T I L I T I E S

Utilities shall be minimally visible and minimally audible from sidewalks and other public pedestrian ways.

Discussion:

Transformer vaults, condensers, electrical and gas meters, as well as overhead transmission lines, can have detrimental effects on the public environment when they are placed in visible and audible locations. Careful siting and design treatment can minimize the detrimental effects of utilities. Siting and design ideas include placing transformer vaults and utility lines underground or within parking garages, placing equipment away from public spaces, and using landscaping to screen views of utility items.



P EDESTRIAN CIRCULATION

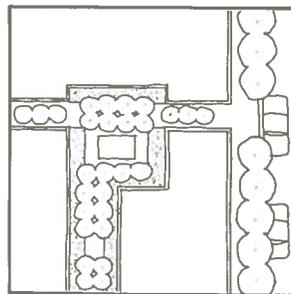
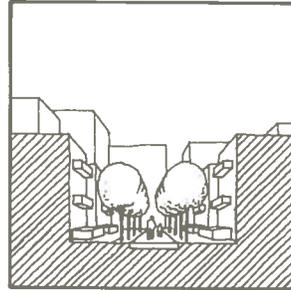
Pedestrian ways which are not contiguous to streets shall include amenities suitable to a pedestrian environment.

Discussion:

Pedestrian paths separate from streets should have a design treatment that is particular to a walking environment. The scale and landscaping of the spaces should relate to the individual person and small groups of people walking and sitting. Surrounding buildings should be designed to respect the privacy of interior uses which are adjacent to pedestrian ways, especially for residential uses. Hardscape and softscape treatments of pedestrian ways should be appropriate to the urban or suburban character of the setting.



Pedestrians walking through the landscaped campus of Bank of America, Concord Center.



PARKING GARAGE HEIGHT

Parking garages shall be as low as possible, especially at street edges. The height of a parking garage shall not be greater than 45 feet above grade, unless it is surrounded for its full height by non-parking uses.

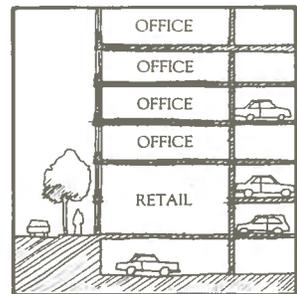
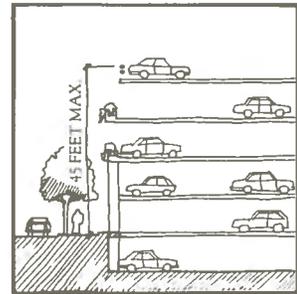
Discussion:

Parking garages are a necessary convenience for the life of a downtown, but their visual impact should be minimized to the greatest extent possible. The major public views of buildings by pedestrians and motorists are from the street, and therefore the primary area of visibility of parking garages is at street edges. Structured parking can be built underground or enclosed within other uses to minimize its visibility from the street.

Underground parking levels may be used in addition to the height permitted above grade to make a larger overall structure. If parking levels above grade are enclosed for their full height within a building or building complex, then there is no parking height restriction, except as defined in the City's zoning ordinance. A building enclosure at the street edges of a parking garage can include retail, office, housing, and other non-parking uses.



The Bank of America parking garage is four levels above grade and about 45 feet tall.



PARKING GARAGE TREATMENT

Parking garage facades shall be enhanced with design treatments and street level uses, as stated in the “Street Level Uses” and “Sidewalk Landscape” guidelines, to improve their appearance and minimize their bulk. Exterior materials shall be harmonious with surrounding buildings, especially adjacent buildings of the same development project.

Discussion:

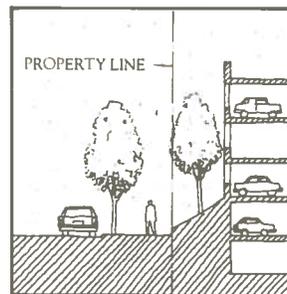
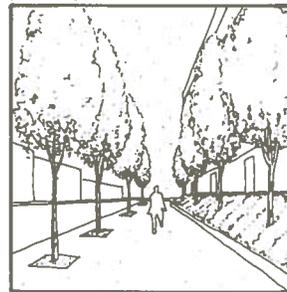
The architectural treatment of parking garages is important for the downtown streetscape, since garages are seen primarily by street level pedestrians and motorists. The placement of retail shops along street frontages with colonnades, awnings, and controlled retail signage is the most effective treatment to enrich the grade and second levels of a parking garage.

Landscaping is also effective. If a garage is set back from the property line, then berms, trees, and climbing plants along the street can screen views of open parking garages. If built at the property line, stepping back of upper floors and placing planters along floor edges can improve appearance and reduce apparent size. Planting at the top edge softens a structure’s profile as seen from the street.

Building materials and the scale of exterior openings should be compatible with buildings in the same development project and should fit with surrounding architecture. Features such as windows, cornices, and one or two story colonnades can help to achieve compatibility. Openings at street level for vehicular access should avoid crossing major pedestrian paths.



The treatment of the parking garage includes shops at street level, glazed openings, and trellises.



PARKING GARAGE ROOFS

Roofs of parking garages, which can be seen from tall buildings, elevated highways, BART trains, and other high vantage points shall be treated to make them as visually attractive as possible.

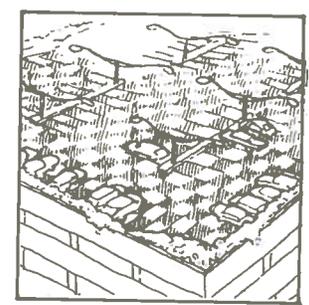
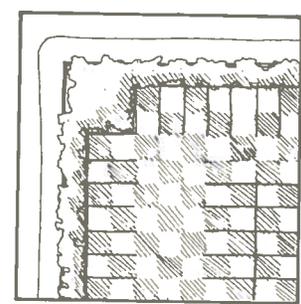
Discussion:

Planting, lighting, shading devices, and paving are elements that can be used to improve the visual quality of parking garage roofs. The use of planters with trees and shrubs, particularly at the edges of a structure, can soften the appearance of a roof, while also providing needed shade for pedestrians and parked cars. Trellis structures can offer additional shade and amenity. Paving with patterns of color can visually enliven large surface areas. Good lighting can improve safety and also be a visual amenity.

This guideline can apply to parking garages below grade as well as above grade. The roof of a structure below grade is often an urban public open space. As such, the roof should be treated as a plaza or courtyard with spatial definition, planting and hardscape of a quality befitting its urban public role.



The City parking garage has a painted pattern on its roof.



BUILDING ROOFS

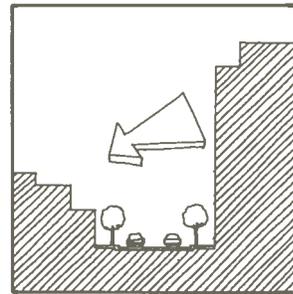
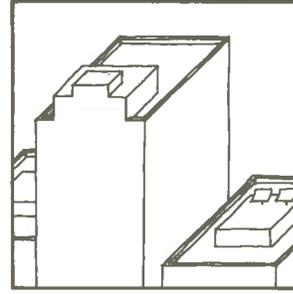
The design of a building roof shall be integrated with the overall design of a building and shall enhance the surrounding downtown roovescape.

Discussion:

As an individual building in the downtown needs to be designed within its streetscape context, so a building roof should be designed within its roovescape context. Roofs of buildings are often the repository of mechanical and electrical equipment, such as antennae, condensers, ducts, cooling towers, and elevator penthouses. When seen from the street, from buildings, or other vantage points, especially when proximate, building roofs with numerous indiscriminately placed pieces of equipment can be quite ugly. It is important to incorporate the various elements of a roof into the building design, so that the building is perceived as a unified design, regardless of the height of the viewer's vantage point. Elements of a roof design requiring design consideration can include flat or sloped roofing materials, parapet walls, equipment, and elevator penthouses. Special features might also be present, such as a helipad, a large dish antenna, a landscaped deck, or accent lighting for the top of the building.



Salvo Pacheco Square has a varied roovescape with equipment nestled between sloping roof sections.



DESIGN REVIEW PROCESS

THE PROCESS AND THE REQUIREMENTS

Design review for the Downtown Business District of Concord is performed by the City's Design Review Board (DRB), which is assisted by City staff, primarily the Planning Department. Review of individual projects occurs at five stages in the design process and the DRB relies upon assistance from the City staff at each stage. City decisions result from the continuous interaction of project development entities, design professionals, the Design Review Board, Redevelopment Agency Board/City Council members, and the City staff. Design review submissions are made to the Planning Department.

Design review focuses attention upon architectural, planning, and urban design issues within the processes of city redevelopment. Many people with varying agendas and schedules are involved in planning and implementation of downtown development projects. In large projects the complexity can be staggering, the duration long, and the actors numerous. The Design Review Process has been established in order to have an efficient and effective method to assess architectural design from project inception to completion. The process allows the City to ensure excellent design quality in the downtown, both in individual projects and in the total environment.

Some projects require full design review, some require a limited review process, and some do not require review at all. Development entities should check with the Planning Department to determine the extent of design review requirements for each proposal. Renovation projects with no spaces or surfaces accessible or visible to the public are exempt from design review. Small projects, such as retail tenant remodeling, and projects with minimal exterior and public interior work, may require limited design review.

The review process consists of five stages of review with a "milestone" event at the end of each stage. The stages correspond to phases of the standard architectural design process, from first concepts to final construction. The First Milestone is at the end of Conceptual Design, which occurs midway through the Schematic Design phase. The Second Milestone is at the end of the Schematic Design phase. The third is at the completion of the Design Development phase,

the point at which all major design and cost decisions should have been made for a project. The milestone for the fourth stage is at the end of Construction Documents and the milestone for the fifth stage is at the end of construction.

The First Milestone review by the Design Review Board (DRB), "Conceptual Review," is not mandatory, unless assistance is sought from the Redevelopment Agency by the developer of a proposed project. For projects seeking Agency assistance, the First Milestone review may follow an Exclusive Right To Negotiate with the Agency and must precede the signing of a Disposition and Development Agreement (DDA) between the Agency and the developer. The DDA includes a provision which states that all required City approvals must be obtained for a project by the developer. The plan submitted to the DRB for the first milestone review and included in the DDA is called the "Preliminary Development Plan." The role of the DRB in the First Milestone review is to make comments in an advisory capacity. The DRB comments may or may not be incorporated into the Preliminary Development Plan.

The Second Milestone is a review of the Schematic Design drawings and is known as the "Preliminary Review." It is undertaken regardless of the existence of a DDA. This stage normally takes place prior to Use Permit approval by the Planning Commission. As in the case of the First Milestone, there is no approval of these plans by the DRB. Comments are made by the DRB members in order to convey their thoughts regarding the proposed design and to provide direction for further development of the plans. The Board may recommend that more than one preliminary review session be held prior to the Third Milestone review of a project.

The Third Milestone is known as the "Final Review" (or "Formal Review") of the project plans at the end of the Design Development phase. This is a required City approval, determined by a vote of a quorum of the DRB members. The Third Milestone review follows the granting of a Use Permit by the Planning Commission. The plans approved by the Planning Commission and the DRB are called the "Final Development

Plan” by the Redevelopment Agency, for DDA purposes. As conditions of its final approval, the DRB may stipulate specific design details to be approved by the DRB at a subsequent meeting or by the Planning Department staff. Examples of such items are the final landscape plans and proposed facade materials.

The Fourth and Fifth Milestones, the “Design Check” and the “Construction Check,” are performed primarily by City staff with the DRB only reviewing specific design items. The Fourth Milestone check is completed prior to the City’s issuance of the Building Permit and the Fifth Milestone check is completed prior to the City’s issuance of the Certificate of Final Completion and Occupancy.

The scale of drawings for design review submissions are listed for each stage. The drawings must be submitted at 100% of the listed scale, and perspective drawings must have an image which is at least 9” by 12”. For large projects, which occupy a site with the equivalent of more than one downtown block, drawings of a scale smaller than the listed requirements may be submitted with prior written approval by the Planning Department. The Design Review Application with a complete checklist of requirements may be obtained from the Planning Department.

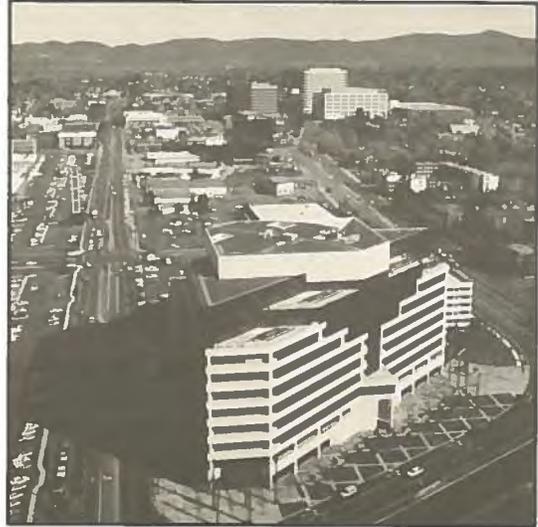


A presentation and public comment are heard in a review session with the City Council.

FIRST MILESTONE: CONCEPTUAL REVIEW

The review of the first design submission to the City is the Conceptual Review. The Conceptual Review corresponds approximately to 50% completion of a project's Schematic Design phase. The First Milestone review is required by the City only if a development proposal requires Redevelopment Agency assistance. The "Preliminary Development Plan" is the name of the plan submitted to the DRB for review and subsequently included in the DDA. The submission requirements include, but are not limited to:

1. Site plan at not smaller than 1" = 20' scale (1:240).
2. Ground floor plan with proximate site area at not smaller than 1" = 8' scale (1:96).
3. Above and below ground floor plans including a roof plan at not smaller than 1" = 8' scale.
4. At least two project sections and two elevations at not smaller than 1" = 8' scale.
5. One exterior, constructed, two-point perspective drawing from a street level viewpoint with the image contained within a 60 degree cone of vision.
6. Tabulation of areas of major exterior and public interior spaces and tabulation of parking spaces by size and type of space.
7. Dimensions of site, parking areas, buildings, and setbacks.



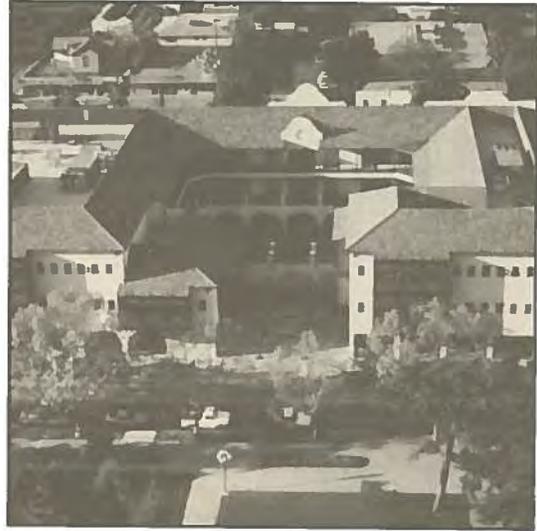
A view towards the east and downtown, looking down Willow Pass and Clayton Roads.

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SECOND MILESTONE: PRELIMINARY REVIEW

The Preliminary Review submission is a set of completed Schematic Design materials, including the required items listed below. The review normally takes place prior to Use Permit approval by the Planning Commission. The DRB can recommend that more than one session be held for this review. The submission requirements include, but are not limited to:

1. Site plan at not smaller than 1"=20' scale (1:240).
2. Ground floor plan with proximate site area not smaller than 1"=8' scale (1:96).
3. Above and below ground floor plans including a roof plan at not smaller than 1"=8' scale.
4. At least two project sections and two exterior elevations at not smaller than 1"=8' scale.
5. Two exterior perspective drawings, at least one of which has a street level viewpoint, and, if applicable, one interior perspective drawing of a major public space. The perspectives shall be constructed, two-point drawings; the interior view can be a one-point or two-point. The drawing images shall be within a 60 degree cone of vision.
6. Tabulation of areas of major exterior and public interior spaces and tabulation of parking spaces by size and type of space.
7. Dimensions of site, parking areas, buildings, setbacks, exterior spaces, and major public indoor spaces.
8. Material and color selections for exterior walls, exterior hardscape, walls and floors of major interior public spaces.
9. Landscape plan with preliminary plant selections at not smaller than 1"=20' scale.
10. Outline specifications.

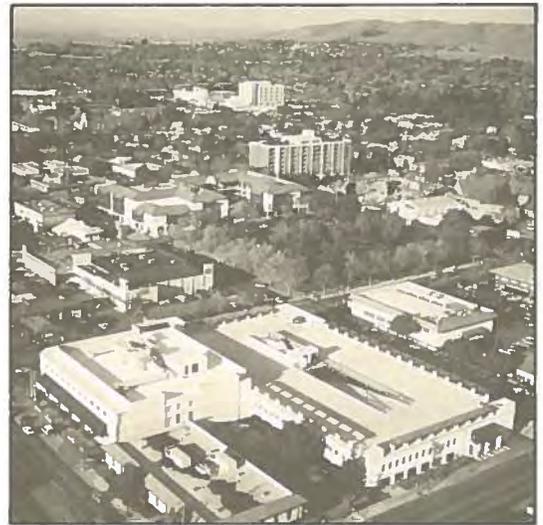


A well-defined urban plaza in Salvio Pacheco Square, facing Todos Santos Plaza.

T HIRD MILESTONE: FINAL REVIEW

Final Review occurs at the end of the Design Development phase of architectural design, at which time all major design and cost decisions for a project should have been made. The design submission for Final Review includes a completed set of the architect's Design Development materials, which update and supplement the Second Milestone requirements (contact the Planning Department for a complete list of requirements):

1. Update Second Milestone: Preliminary Review site plan requirement.
2. Update Second Milestone: Preliminary Review ground floor plan requirement.
3. Update Second Milestone: Preliminary Review requirement for floor and roof plans.
4. Update Second Milestone: Preliminary Review requirements for sections and elevations plus all remaining exterior elevations at not smaller than 1" = 8' scale (1:96).
5. Update Second Milestone: Preliminary Review perspective drawings requirement.
6. Update Second Milestone: Preliminary Review requirement for tabulation of areas and parking spaces.
7. Update Second Milestone: Preliminary Review dimensions requirement.
8. Update Second Milestone: Preliminary Review requirements for material selection plus material and color boards of exterior walls and major interior public spaces, as well as exterior hardscape at the ground plane, at visible roofs, and at usable roof decks. A mock-up panel of each major exterior building wall treatment, in model form at a scale from 1" = 8' (1:96) to 1" = 1' (1:12), may be required by the Planning Department.
9. Update Second Milestone: Preliminary Review requirement for landscape plus landscape sections and elevations at not smaller than 1" = 8' scale, including hardscape, lighting, equipment, furnishings, and planting for on-site and off-site improvements.
10. Update Second Milestone: Preliminary Review requirement for outline specifications, updated and more detailed.
11. Reflected ceiling plans, including soffits, of exterior and major public interior spaces at not smaller than 1" = 8' scale. Spaces such as arcades, colonnades, lobbies, malls, courtyards, and galleries should be included.
12. Interior elevations of major public interior spaces at not smaller than 1" = 8" scale.
13. Graphics and signage: locations, schedules, and samples or manufacturer's literature for exterior and interior public spaces, including parking garage and building identification signs.
14. Lighting schedules with samples or manufacturer's literature for exterior and interior public spaces and parking garages. Lighting locations are to be shown on landscape plans, reflected ceiling plans, and elevations.



An aerial view of Todos Santos Plaza from the Southwest with the Concord Main Office of Bank of America in the foreground.

FOURTH MILESTONE: DESIGN CHECK

The Fourth Milestone: Design Check, occurs at the completion of the Construction Documents phase. The Design Check is performed by the Planning Department, except for specific elements of the submission which may also be checked by the Design Review Board. If the Redevelopment Agency is involved in the project with a DDA, then the Agency will help the Planning Department perform the Design Check. Completed Construction Documents, including final landscape plans, are submitted to the Planning Department and they are checked for conformance with the Third Milestone review. The reviewed documents are given to the Building Department for plan check approval and issuance of a Building Permit. For a development with multiple, phased construction contracts, several Building Permits might be issued, necessitating a Design Check for each permit.

Submission requirements for the Design Check are a complete set of Construction Documents for the construction work being considered, in addition to clarification drawings and text for changes in the design since the Final Review of the Third Milestone.

FIFTH MILESTONE: CONSTRUCTION CHECK

Issuance by the City of the Certificate of Final Completion and Occupancy for a development project is contingent upon a Construction Check by the Planning Department. If the Redevelopment Agency is involved in the project with a DDA, then the Agency will help the Planning Department perform the Construction Check. Change orders will be reviewed and site visits made by the Planning Department for the Construction Check.

Submission requirements for the Fifth Milestone: Construction Check, include construction Change Orders, which affect the appearance or use of the exterior and public interior portions of a project. Clarification drawings and text explaining design changes made since the Fourth Milestone: Design Check, will be submitted upon request of the Planning Department.

For projects over 50,000 gross square feet of built floor area, design review will include approval of a full-scale mock-up of the major exterior wall system, built on the project site. The mock-

up will include the actual materials, finishes, and colors to be used on the project. Approval will be necessary before construction of the exterior wall system commences.

C R E D I T S

City Council and Redevelopment Agency Board

Ron Mullin, Mayor
Colleen Coll, Vice Mayor and Agency Chair
June Bulman, Agency Vice Chair
Diane Longshore
Steve Weir

Design Review Board

November 1986:

David Goldin, Chair
William Richardson
John Nicol
Michael Pastrick
Shadrick Small*

Former Members Since
January 1984:

Carl Campos
Don Rose
Frank Mighetto
Joseph Calibrigo
Charles Carpenter
Christine Callahan*
Lynette Keihl*
Ward Pynn*
Theodora Shea*

*Representatives of the Planning Commission
serving in rotation.

City Staff

Bill Waterhouse, Director of Redevelopment
Peter Hirano, Planning Director

ELS/Elbasani & Logan Architects

Donn Logan, Principal
Frank Fuller, Project Architect

Photograph on back cover: Downtown Concord as the 1940's center of a primarily agricultural community, seen from the southeast.



CONCORD CALIFORNIA

EARLY CALIFORNIA ARCHITECTURE

SPANISH REVIVAL

The Spanish Revival Style was an architectural movement that began in the late 19th century for a colonial style's revivalism and reinterpretation, which drew inspiration from the late 18th and early 19th century Spanish missions in California. This architecture style has been considered to be the regional vernacular architecture style of Southwestern United States, especially in California.

Spanish Revival replicated much of the original Spanish Architecture style's aesthetic, which includes enclosed courtyard, massive adobe wall, tile roofing, and outdoor shade arcades.

Some of the structures in City of Concord that uses Spanish/ Mission Revival Style architecture are the Todos Santos Square in the north side of the Todos Santos Plaza.



Painted Tile



Arches



Arcade



Ornamental Iron work



Tower-like Chimney



Wooden Door



White Stucco Exterior Wall



Terracota Roof Tile



Courtyard

MONTEREY COLONIAL



Cantilever on 2nd Level



Low-pitch Gable Roof



Wood Material on the Cantilever



Adobe Wall

Monterey Colonial Architecture is an architecture style that originated in California. Larkin House, made by a Boston merchant is widely considered as the first of the Monterey Colonial Style.

Monterey Colonial has distinct characteristics, which includes low-pitched gable roof, sometimes displayed different material on the first, the usage of wood post in the balcony and second floor and cantilevered second floor balcony.

The known building that uses Monterey Colonial style in Concord are the Pacheco Adobe House, which is the first structure in Concord.

VICTORIAN

Victorian Architecture refers to architecture style that were used during the reign of Queen Victoria in Britain. This architecture style was brought to United States around 1860s and become very popular at that time.

Many of the city in California adopted Victorian architecture for their building. Some of the city includes San Francisco, Eureka and Alameda.

Victorian Architecture can be divided into several sub-category such as Queen Anne, Italianate, Shingle, and several other. Some of the main characteristic of Victorian architecture including but not limited to ornamental bracket and low roofs (italianate), bay window, steep roof, round or square tower (Queen Anne) and continuous wood shingle, porches, cross gable and irregular roof line (Shingle). Most of Victorian Architecture share material similarity, where they used wood as the main material.



Low Roof of Italianate



Queen Anne's Steep Roof



Shingle Cross Gable



Ornamental Bracket



Round/Square Tower



Shingle Style Porch



Side Bay Window



Queen Anne Bay Window



Irregular Roof Line

MISSION REVIVAL



Smoot Stucco



Arched Entry and Window



Arcade



Quatrefoil Window



Exposed Rafter



Gabled Tile Roof

Mission revival is an architecture style that emerged in late 19th Century, which inspired by early Hispanic Mission architecture style in California. This style emerged as a response of the actual mission's fading condition and restoration, which in turn bring nostalgia to the public.

Mission Revival replicated much of the original Mission style's aesthetic, which includes enclosed courtyard, massive adobe wall, tile roofing, and outdoor shade arcades, while integrating these characteristic with more modern material to improve the structure's strength. Mission Revival style bear many resemblance to Spanish Revival. Some of the shared characteristic between these two style includes the outdoor shade arcade and courtyard.

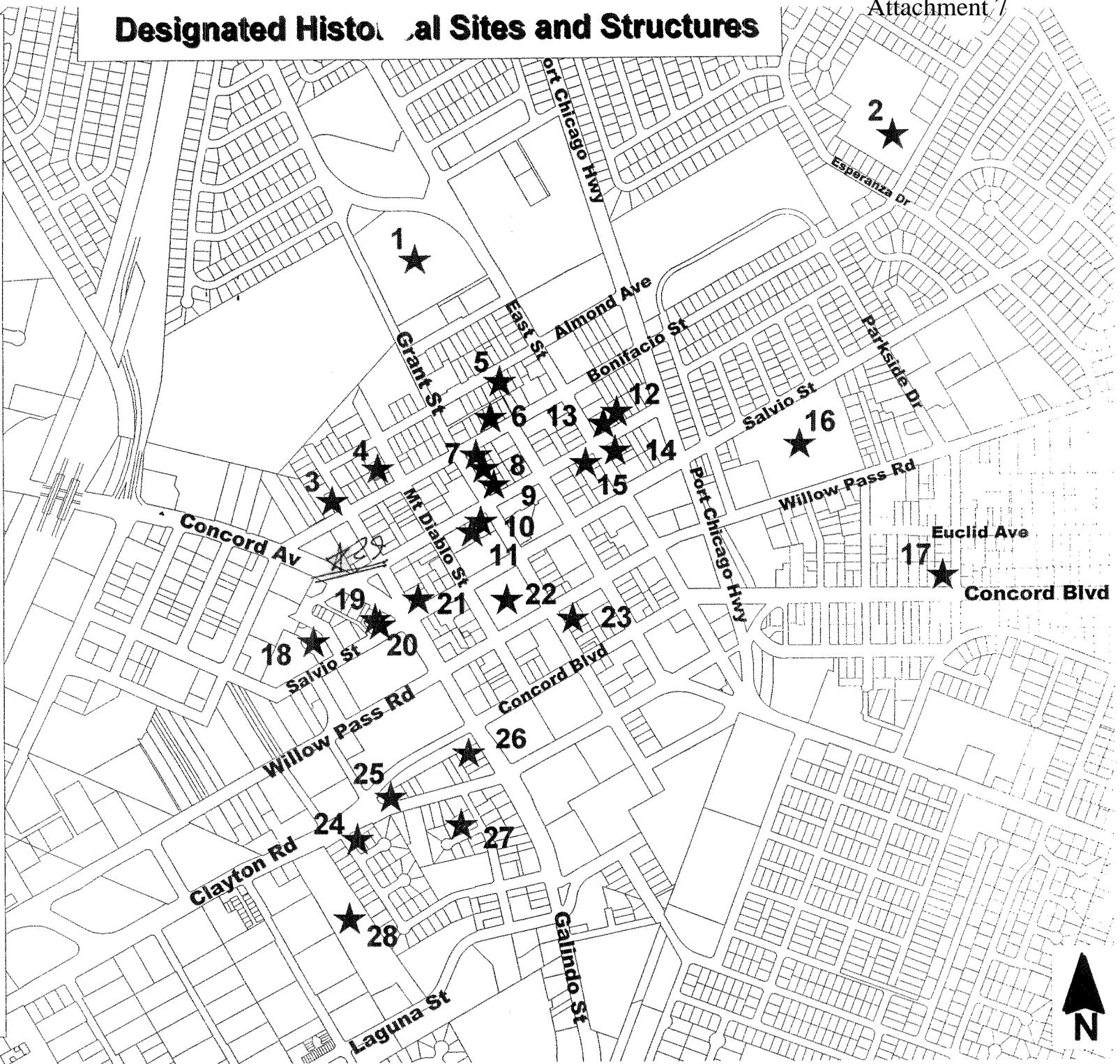
Attachment 6 – City of Santa Barbara - El Pueblo Viejo Design Guidelines

This attachment is too large to include in the “Discussion of Early California Architecture for the Downtown” so the link to this sight is available at the website below:

https://www.dropbox.com/s/8fe2077o4kbofes/Att%204-CitySantaBarbaraEPV_Guidelines.pdf?dl=0

Designated Historical Sites and Structures

- 1. Mt Diablo High School
- 2. Maltby Mansion
- 3. Kelly House
- 4. Webb-Soto House
- 5. Nunez House
- 6. Bolla House
- 7. Alves House
- 8. Neustaedter House
- 9. Bibber House
- 10. 1930s Apartment House (Rosal Apartments)
- 11. Elworthy-Keller
- 12. Ginochio-Accinelli House
- 13. Elworthy House
- 14. Barnett House
- 15. Maltby-McKinnon House
- 16. Concord Elementary School
- 17. Gieselhart House
- 18. Salvio Pacheco Adobe
- 19. Old Fire House
- 20. Perry House
- 21. Foskett-Elworthy Bldg
- 22. Todos Santos Plaza
- 23. County Fire House
- 24. Eddy House
- 25. Ivey House
- 26. Masonic Hall
- 27. Francisco Galindo Home
- 28. Keller House



29. BEEBE HOUSE

CITY COUNCIL COMMITTEE

SPECIAL MEETING

HOUSING & ECONOMIC DEVELOPMENT

Dan Helix, Chair

Ron Leone, Committee Member

4:45 p.m., Monday, September 28, 2015

(Please note earlier start time)

**Building D, Permit Center Conference Room
1950 Parkside Drive, Concord**

- ANNOTATED AGENDA -

ROLL CALL: All present

STAFF PRESENT: John Montagh, Economic Development and Housing Manager; Laura Simpson, Planning Manager; Afshan Hamid Associate Planner; Jovan Grogan, Deputy City Manager, and Victoria Walker, Director of Community and Economic Development

MEMBERS OF THE PUBLIC WHO ADDRESSED THE COMMITTEE: None

PUBLIC COMMENT PERIOD: None

1. **DISCUSSION** – of Changeable Electronic Variable Message Signs (CEVMS). Report by Afshan Hamid, Associate Planner.

ACTION: Afshan Hamid provided an update on the H&ED Committee's request for Changeable Electronic Variable Message Signs (CEVMS). It is a multi-faceted issue that needs to consider benefits and impacts. Benefits to the City may include: 1) new opportunities for promotional and/or commercial advertising for local businesses; 2) Public Service Announcements (PSA) of public safety, branding, tourism and downtown events, in limited quantities and timeframes, through a licensing agreement; 3) an annual fee to the City; and 4) utilizing the sign as a gateway feature for the City of Concord. The Concord Municipal Code, prohibits freeway signs and Section 18.180.080 (C) prohibits electronic message display signs and variable intensity. This code section also prohibits Brilliant Lights (18.180.080 D), any sign with brilliant lighting that conflicts or interferes

CITY COUNCIL COMMITTEE

with traffic, both vehicular and pedestrian, from a public safety standpoint or affects adjacent properties. Ms. Hamid cited legal concerns such as freedom of expression and the First Amendment, where sign ordinances may regulate the time, placement and manner of signage only. The City's current sign regulations allow on-site, which are signs to direct attention to a business or service on the property where the sign is placed, such as a shopping center entrance sign. The sign code currently prohibits off-site or off-premises signs, which direct attention to a business, etc. at a location different from where the sign is located, such as billboards. If a CEVMS is leased on a private property, the sign would allow any commercial business in any location to promote and advertise, thus becoming an off-premise sign which is currently not allowed. Research on driver distraction and safety was cited, with Caltrans 2012 Study and a PAS report through American Planning Association. In terms of Economic Benefits, within Constitutional free speech protections, the sign may advertise all competitive businesses, retail and products, including those from surrounding and distant communities.

Policy Issues:

If a CEVMS were to move forward, the larger issues of public policy to address are:

- Does the proposed CEVMS meet the purpose of the sign regulations?
- Allowing an off-premise sign on a private leased property.
- Regulation of content.
- Legal concerns of equitability.
- A public gateway sign on a private property.
- Evaluating the sign from a safety and traffic perspective.

Staff contacted or visited local communities to better understand the aesthetics, location, and types of freeway-oriented signs. Two communities visited were Benicia and Martinez. Both communities recently approved digital signs along the I-680 north in the industrial parkway zoning district. The sign has a static Benicia City logo topping a CEVMS. Staff observed eight changing images in this CEVMS with commercial advertisements from non-Benicia communities. Pleasant Hill, Walnut Creek and San Ramon prohibit electronic signs and billboards as of right.

Ms. Hamid commented that if a sign amendment proceeds, notice must be given within a 500 foot radius including residential districts. Input on this issue would be needed from residents in City of Pleasant Hill and the City of Concord through public hearings at the Planning Commission.

Committee Member Ron Leone stated that the idea for a CEVMS was his brain child and he was interested in showcasing local events and branding the City of Concord. He was also interested in pursuing it with limited and regulated Cal-Trans approval so that Pleasant Hill does not pursue one. Chairman Dan Helix cited safety concerns with driver distraction. He applauded Committee Member Ron Leone's initiative, however based on research he was not in support of pursuing a CEVMS further. He has seen several in Southern California, and commented that the changing signs are distracting. He would

CITY COUNCIL COMMITTEE

prefer to have driver's focused on driving and would not like to see this move forward to full Council.

2. **DISCUSSION** – of Early California Architecture for the Downtown. Report by Joan Ryan, Senior Planner.

ACTION: Joan Ryan presented a report sharing the background of the Downtown Specific Plan, a summary of the current design guidelines and the reasoning behind the implementation strategy included within the plan to further explore the Early California theme. She then introduced Afshan Hamid, Associated Planner, who provided a powerpoint presentation providing a brief history of the mixture of architectural influences that have combined over time, resulting in a variety and melding of architectural styles within California. These influences have included Spanish Revival, Monterey Colonial, Victorian, and Mission Revival architectural styles. Each of these styles has representations within Concord and she highlighted those buildings within the downtown and North Todos Santos district. Staff then requested the Committee's input and direction as to how to move forward to better define this issue in terms of: 1) a preference for more defined architectural design guidelines or standards; 2) the geographic boundary for which projects should be subject to such guidelines or standards; and 3) the type of development thresholds that would be subject to the guidelines or standards.

The Committee discussed the core downtown as a jewel where they would like to see the past reflected. One graphic was provided to staff noting a desire for historical street signs. The Committee indicated a desire to take the next step in establishing a core district and linking that with BART. The Committee noted there may be federal grants available for a tram to improve transportation options and encourage walking. The Committee indicated they are interested in the input of the public, particularly developers in crafting guidelines or standards and invited their comments.

Members of the public provided input and emphasized the need for flexibility, and the desire for variety to provide attractive curb appeal. They stated the need for awareness of cost considerations in preparing any guidelines or standards and requested the need for a clear process for review of projects. They recommended a scaled gradient, where requirements are more substantial in the core downtown around Todos Santos Plaza and reduced as you move away from the core. One caution was to remove roadblocks and reduce costs to renovation/remodels or there is the risk of buildings becoming decrepit.

The Committee provided direction to staff as follows:

- The need to establish boundaries with the immediate area surrounding Todos Santos Plaza as the central focus, along with the primary street corridors and requested staff return with recommended boundaries for an inner core vs. the outer core.
- The need for flexibility in implementation of design guidelines, respecting a variety of styles, and clarifying the process for review.
- The need to respect cost considerations, including a hierarchy of guidelines for the inner vs. outer core and establishing some type of incentive program for revitalization.

CITY COUNCIL COMMITTEE

- The need for sketches as well as pictures to represent desired architectural elements.
- The desire to create a transit corridor between BART and Todos Santos Plaza to improve traffic connectivity, with a tram or similar to move people quickly from BART.

3. **ADJOURNMENT** at 6:45 p.m.

Reviewed by:



Jovan Grogan, Deputy City Manager

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Distribution: City Council
Valerie Barone, City Manager
Jovan Grogan, Deputy City Manager
Victoria Walker, Community & Economic Development Director
John Montagh, Economic Development & Housing Manager
Laura Simpson, Planning Manager
Joan Ryan, Senior Planner
Afshan Hamid, Associate Planner
Administrative Services

**REPORT TO COUNCIL COMMITTEE ON
HOUSING AND ECONOMIC DEVELOPMENT****TO HONORABLE COMMITTEE MEMBERS:**

DATE: October 26, 2015

SUBJECT: DISCUSSION OF TODOS SANTOS DESIGN GUIDELINES**Report in Brief**

On September 26, 2015, the Housing and Economic Development Committee met to initiate a discussion regarding the incorporation of early California architecture into the downtown. The Downtown Concord Specific Plan currently includes a chapter on Design Guidelines to provide guidance to potential property owners, developers, the City's Design Review Board and staff to evaluate the architectural design for new development or exterior building remodel projects in the downtown. Based on the direction provided at the last meeting, the Committee is interested in moving forward with the preparation of more defined guidelines. This meeting takes the next step in discussing the work plan to move this effort forward.

Background

The City Council unanimously adopted the Downtown Concord Specific Plan (Specific Plan) on June 25, 2014, which includes a chapter on Design Guidelines that focuses on urban form, massing and character, ground floor treatment, relating facades to the public realm, vehicular access and building design including parking, servicing and private open space. However, the current guidelines are general in nature and are not overly prescriptive.

On September 28, the Housing and Economic Development Committee received a staff report that discussed the idea of creating more specific architectural guidelines or policies to facilitate architecture reflective of California's history within the Downtown Specific Plan area. The staff report shared the background of the Downtown Specific Plan, a summary of the Plan's design guidelines, the purpose of the Plan's implementation strategy to further explore the early California theme, and a power point presentation summarizing a brief history of early California architecture. The architectural influences have been combined over time, resulting in a variety and melding of architectural styles within California. These influences have included Spanish Revival, Monterey Colonial, Victorian, and Mission Revival architectural styles, and all of these styles have representations within Downtown Concord and the North Todos Santos zoning district.

The Chair and Vice Chair of the Design Review Board and Planning Commission were invited to the meeting, as well as downtown property owners and developers. Approximately 15 interested persons attended the meeting, and many provided public comment.

The Committee discussed the downtown area as a jewel where they would like to see the past reflected. Developers and property owner comments were generally supportive of the idea of new development incorporating an early California theme. However, they also provided input stating their need for design flexibility and a desire for variety to provide attractive curb appeal and address the needs and desires of current and prospective tenants. They emphasized the need for cost sensitivity, and stated they

would like the City to provide incentives and requested the need for a clear process for review of projects. They also suggested that new architectural requirements to incorporate the early California architectural themes could be more focused in the central area surrounding Todos Santos Plaza, with less emphasis away from that central area. There was support to create a more focused Todos Santos district that centers on the Plaza and neighboring streets.

The Committee provided direction to staff as follows:

- 1) the need to establish boundaries where the development would be subject to new design guidelines, with the immediate area surrounding Todos Santos Plaza and its main street corridors as the central focus, including a hierarchy of guidelines for the inner vs. outer core;
- 2) the need for flexibility in implementation of design guidelines, respecting a variety of styles, and clarifying the process for review;
- 3) the need to respect cost considerations, and seeking some type of incentive program;
- 4) the need for sketches as well as photos in the development of the design standards, to encourage design creativity; and
- 5) the desire to create a transit corridor between BART and Todos Santos Plaza to improve traffic connectivity, with a tram or similar facility.

Discussion

The following outline of a work program for the preparation of the Todos Santos Design Guidelines (specific to the downtown) has been prepared for discussion with the Committee to provide guidance based on the comments provided at the September 28 Council Committee meeting.

Boundary Designations for the Todos Santos Design Guidelines

Direction for a hierarchy of guidelines distinguishing between an “inner” vs. “outer” core requires clear boundary designations. Staff believes the core focus should be upon those buildings located immediately around Todos Santos Plaza. However, it could be argued that an expansion of that “inner core,” would benefit the City by including properties west along the Salvio Street corridor, as reflected on Attachment 1. Expansion of the “inner core” along Salvio Street is justified in that Salvio Street was historically Concord’s main street. This would serve to link Todos Santos Plaza with one of the oldest buildings in Concord, the Salvio Pacheco Adobe House, located on the northwest corner of Salvio and Adobe Street. Attachment 2 reflects the City’s historical downtown buildings. The area surrounding the Salvio Pacheco Adobe House including Clay Alley was highlighted within the Specific Plan as a special opportunity area to create a different and smaller scale retail/outdoor dining environment. Development of this area could provide a linkage over time between the downtown core and Park and Shop.

Outside of the “inner core”, the “outer core” has been defined on Attachment 1, generally by Port Chicago Highway and the BART tracks on the east, the downtown BART station on the south, up to Park and Shop on the west, and just north of Pacheco Street on the north. The “outer” core thus, does not take in the entire downtown Specific Plan boundary, which includes substantial acreage, but instead, includes key areas where staff believes development will likely happen in the near to mid-term.

Enhanced and More Defined Guidelines

As discussed at the September 28 meeting, California's early architecture can be viewed as a mixture of architectural influences that have combined over time, evolving based on a variety and melding of architectural styles. These influences have included Spanish Revival, Monterey Colonial, Victorian, and Mission Revival architectural styles. During the last Committee meeting, an emphasis was placed on maintaining flexibility while retaining the City's historical past. Below is a draft outline for the proposed Todos Santos Design Guidelines, based on the input provided at the September 28 meeting.

Todos Santos Design Guidelines

Purpose: To create guidelines that support authenticity, flexibility and retain Concord's historical context.

- I. Introduction and Background
- II. California Architecture: A brief history
- III. Approach
 - Designation of Todos Santos district (Inner vs. Outer Core)
 - Character Defining Features of the District
 - Hierarchy of Guidelines
 - o Inner Core
 - o Outer Core
- IV. Compatibility Guidelines
 - Building Massing
 - Height and Setbacks
 - Presentation to the Public Realm
- V. Architectural Design Elements
- VI. Thresholds
 - New Buildings
 - Remodels and Renovations
- VII. Timing and Process
 - Design Review Board

The focus of the guidelines will be to provide guidance to potential property owners, developers, the City's Design Review Board and staff on acceptable architectural designs for projects in the Todos Santos District as outlined in Attachment 1. The guidelines will describe the desired urban form and massing, setbacks and ground floor treatment, as well as provide further clarity and examples/tools to assist developers and architects in the design of their projects to quickly navigate the design review process. The intent is that the guidelines would provide flexibility, while at the same time providing further clarity as to the architectural themes that resonate with Concord's historical context. Staff intends to promote authenticity in design character so that design buildings do not appear cartoonish in nature, but instead reflect the rich and diverse architecture from California's history.

Thresholds

Staff proposes that all new buildings within the designated boundary would be subject to the Todos Santos Design Guidelines. In addition, building expansions of greater than 50 square feet would be subject to the Guidelines. Renovations and rehabilitations with a project valuation of \$10,000 or greater would also be subject to the Design Guidelines.

Corridors Streetscape Project

On January 13, 2015, the City accepted a \$250,000 Priority Development Area planning grant from the Contra Costa Transportation Authority for the Downtown Corridors Plan. ARUP was retained as the consultant on the project to further evolve the concept of the “Green Streets Framework” that conceptualized a rectangular frame that would provide enhanced multi-modal circulation between four key locations in Concord’s downtown, with the corners represented by the Downtown BART Station, Todos Santos Plaza, the Park and Shop Shopping Center and Ellis Lake Park. The Corridors Streetscape Project is also intended to prepare design development drawings for a complete streets project to enhance three critical street corridors that comprise the “frame” including Grant Street, Salvio Street and Oak Street to better connect residents living and working in the downtown to transit, housing, retail, and jobs.

Staff believes it is important that the Corridors Streetscape Project should be closely coordinated with the Todos Santos Design Guidelines. The Corridors Project scope of work includes looking at existing conditions, opportunities and constraints within the public right-of-way for the three corridors and developing conceptual designs for streetscape and intersection along the three corridors. Design guidelines for lighting and street furniture, storm water facilities, provisions for pop-up/temporary commercial uses and accessibility will also be prepared and should be completed by July 2016. The first public meeting is planned for November 2 at 6 p.m. at Salvio Pacheco Plaza, Suite V (2151 Salvio Street).

Timeline and Process

Staff proposes to prepare the Todos Santos Design Guidelines in-house with existing staff within the Planning Division and anticipates a timeline of approximately 9 months. The timeline would allow for public outreach to obtain feedback regarding staff’s progress and coordination with the Design Review Board and Ad-Hoc Committee at appropriate milestones, prior to review and adoption by the City Council. The project would be initiated in November 2015 and would continue on a parallel track with the Corridors Project, such that the two projects can be coordinated.

Program Incentives

The City previously had a Downtown Facade Improvement Grant Program that was funded through Redevelopment Funds. The program provided the impetus for a variety of downtown facade improvements by providing funding of up to 2/3 of the approved project costs, with a maximum grant amount of \$50,000. This program (grant funding and money for administration) as well as other activities of the Concord Redevelopment Agency were eliminated in 2012 when the State of California dissolved redevelopment agencies statewide. Re-establishing a similar program will be a challenge without a dedicated funding source. Staff is not aware of a Federal or State grant program to support local façade improvements.

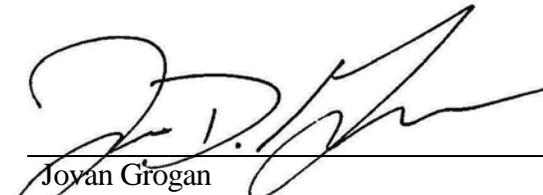
Next Steps

Staff is looking for input and guidance from the Committee as to the general approach for moving forward. These include:

- 1) Designating a boundary for the Todos Santos Design Guidelines and defining the inner vs. outer core
- 2) Providing input on the draft outline for the Design Guidelines document
- 3) Indicating agreement on proposed thresholds for the Design Guidelines (new buildings and renovations/remodels)
- 4) Confirming proposed process

Public Contact

The agenda item was posted. Developers with an interest in the downtown have been notified.



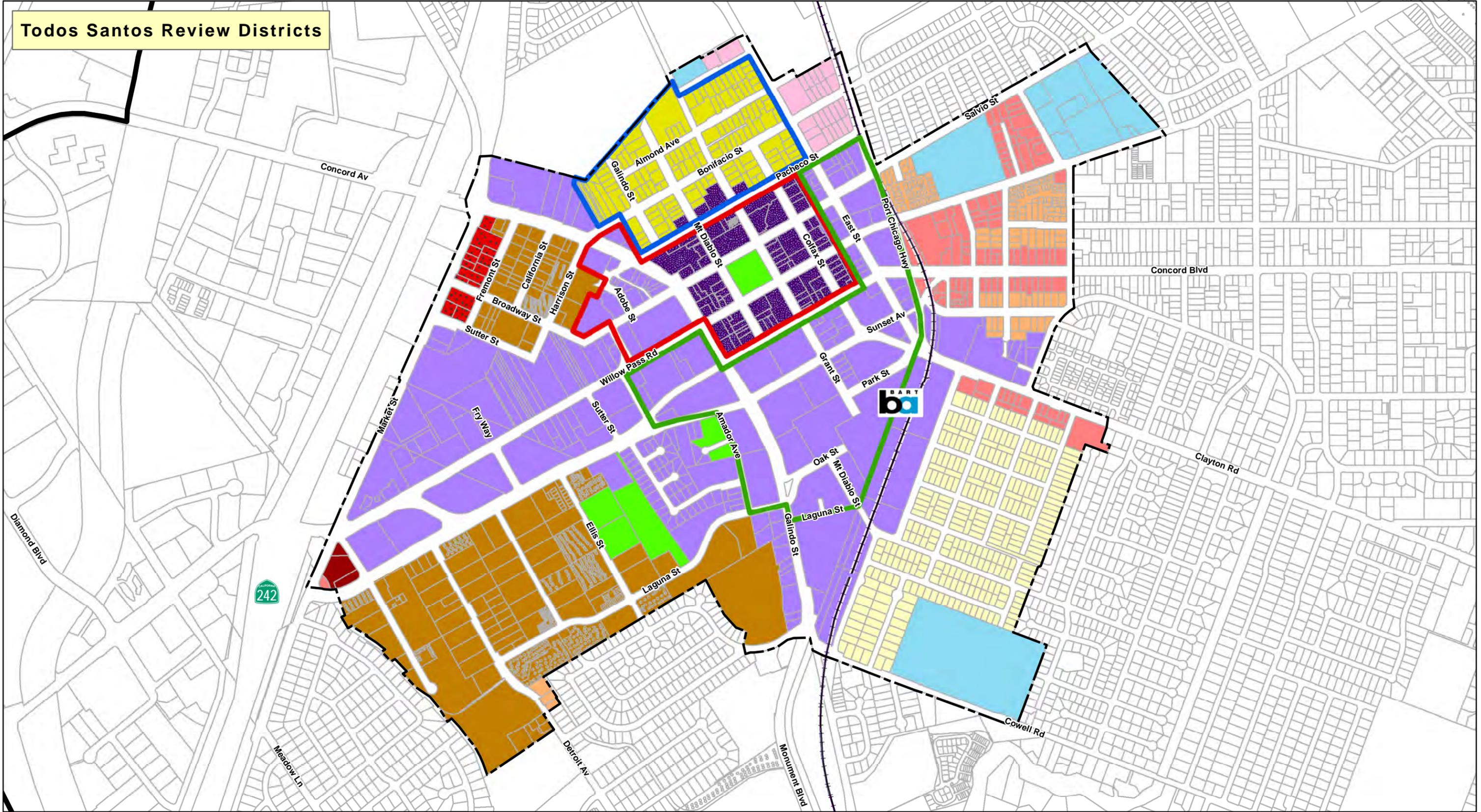
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Deputy City Manager
Jovan.grogan@cityofconcord.org

Prepared by: Joan Ryan, AICP
Senior Planner
joan.ryan@cityofconcord.org

Reviewed by: Laura Simpson
Planning Manager
Laura.simpson@cityofconcord.org

- Attachment 1 – Boundary designation map for Todos Santo Design Guidelines
Attachment 2 – Concord Historical Structures, map and listing

Todos Santos Review Districts



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Legend

- Priority Development Area
- City Limit
- BART Station

Todos Santos Review Districts

- Inner Core
- Outer Core
- North Todos Santos

General Plan 2030 Land Use

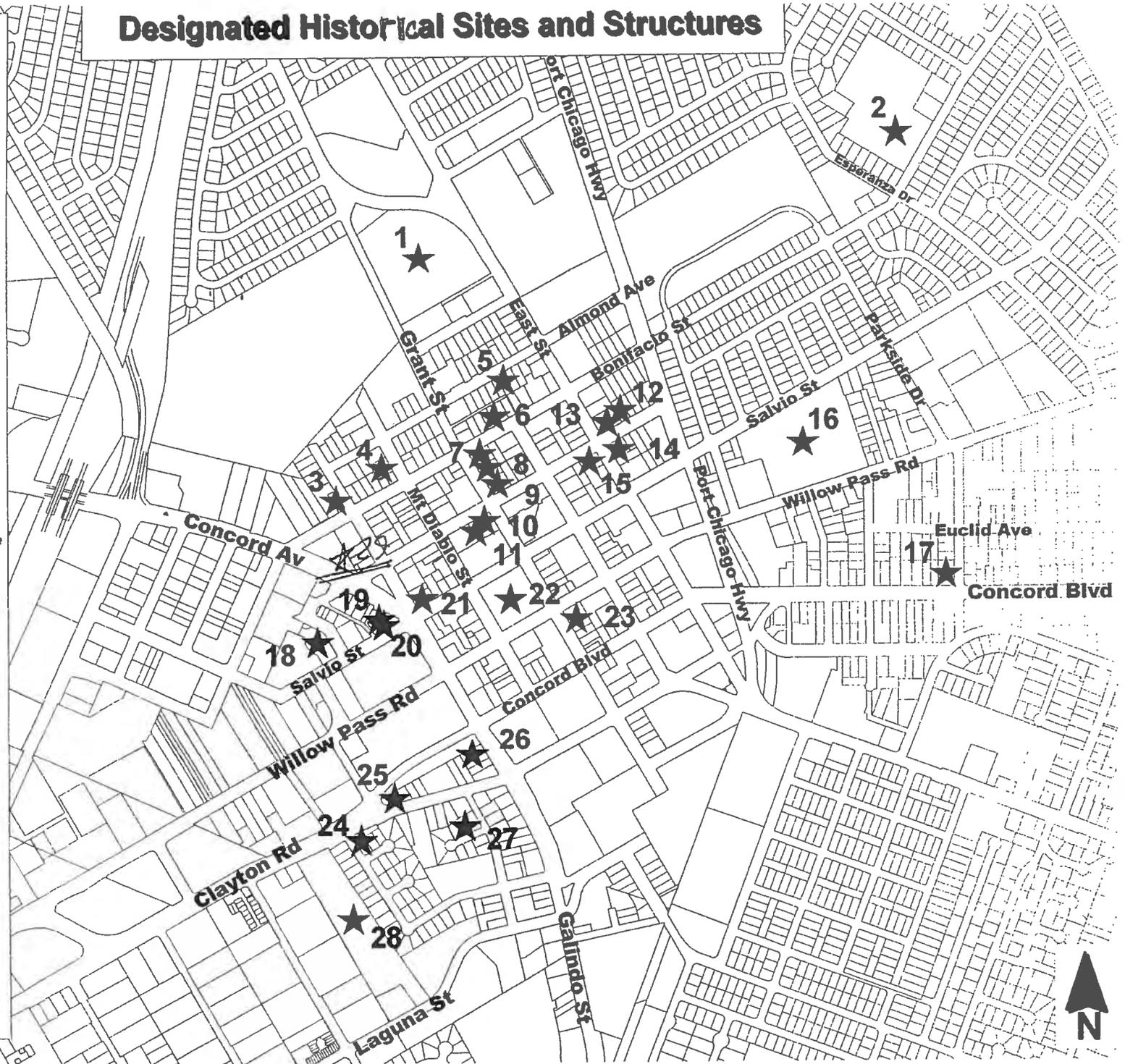
- Low Density Residential (LDR)
- Medium Density Residential (MDR)
- High Density Residential (HDR)
- North Todos Santos (NTS)
- Community Office (CO)
- Commercial Mixed Use (CMU)
- Regional Commerical (RC)
- Service Commerical (SC)
- Downtown Pedestrian (DTPD)
- Downtown Mixed Use (DTMU)
- Public/Quasi-Public (PQP)
- Parks and Recreation (P)



Designated Historical Sites and Structures

1. Mt Diablo High School
2. Maltby Mansion
3. Kelly House
4. Webb-Soto House
5. Nunez House
6. Bolla House
7. Alves House
8. Neustaedter House
9. Bibber House
10. 1930s Apartment House (Rosal Apartments)
11. Elworthy-Keller
12. Ginochio-Accinelli House
13. Elworthy House
14. Barnett House
15. Maltby-McKinnon House
16. Concord Elementary School
17. Gieselhart House
18. Salvio Pacheco Adobe
19. Old Fire House
20. Perry House
21. Foscett-Elworthy Bldg
22. Todos Santos Plaza
23. County Fire House
24. Eddy House
25. Ivey House
26. Masonic Hall
27. Francisco Galindo Home
28. Keller House

29. BEEBE HOUSE



CITY COUNCIL COMMITTEE

HOUSING & ECONOMIC DEVELOPMENT

Dan Helix, Chair

Ron Leone, Committee Member

5:30 p.m., Monday, October 26, 2015

**Building D, Permit Center Room
1950 Parkside Drive, Concord**

- ANNOTATED AGENDA -

ROLL CALL: All present

STAFF PRESENT: Jovan Grogan, Deputy City Manager; Victoria Walker, Director of Community & Economic Development; Laura Simpson, Planning Manager; Joan Ryan, Senior Planner

MEMBERS OF THE PUBLIC WHO ADDRESSED THE COMMITTEE: None

PUBLIC COMMENT PERIOD: None

1. **DISCUSSION** – of Todos Santos Design Guidelines. Report by Joan Ryan, Senior Planner.

ACTION: The Council Committee provided the following direction, with a request for a follow-up meeting, prior to initiating the document.

- 1) Expand the boundary for the Todos Santos Design Guidelines to include the following areas within the inner core: a) the Downtown Pedestrian zoned area on the north side of Pacheco Street; b) the eastern portion of the blocks between Colfax and East Streets (between Pacheco and Concord Blvd.); c) an area between Colfax St. and Grant St. (between Concord Blvd. and Park St.); and d) a portion of Salvio Street between Mira Vista and the western edge of the boundary.
- 2) Confirmed the draft outline that would be further detailed once the project is initiated.

CITY COUNCIL COMMITTEE

- 3) Confirmed thresholds – All new buildings and renovations of \$10,000 or greater in exterior improvements would be subject to the Guidelines.
- 4) Discussed Incentives – Suggested fee waivers as a potential method to provide incentives which will be further studied during the preparation of the Guidelines.
- 5) Process – Established staff will initiate preparation of the design guidelines in November with a 9-month timeline to remain on a parallel path for coordination purposes with the Corridors Project, which is studying and preparing streetscape designs and design guidelines for Oak Street, Grant Street and Salvio Street.

2. **ADJOURNMENT** at 6:50 p.m.

Reviewed by:



Jovan Grogan, Deputy City Manager

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Jovan Grogan, Deputy City Manager
Victoria Walker, Community & Economic Development Director
Laura Simpson, Planning Manager
Joan Ryan, Senior Planner
Administrative Services

**REPORT TO COUNCIL COMMITTEE ON
HOUSING AND ECONOMIC DEVELOPMENT****TO HONORABLE COMMITTEE MEMBERS:**

DATE: November 16, 2015

SUBJECT: DISCUSSION OF TODOS SANTOS DESIGN GUIDELINES**Report in Brief**

On October 26, 2015, the Housing and Economic Development Committee met to discuss the preparation of the Todos Santos Design Guidelines for the downtown. The Committee provided feedback on the general approach for moving forward including (1) designation of a boundary for which projects would be subject to the guidelines, and defining an inner and outer core, whereby guidelines would be hierarchical; (2) a draft outline for the document; (3) the proposed thresholds by which new and renovated buildings would be subject to the Guidelines; (4) discussion on incentives; and (5) confirming the process. The Committee requested further discussion on Item 1 and 4.

Background

During the last Council Committee meeting, the following direction was provided, with a request for a follow-up meeting, prior to preparing the Todos Santos Design Guidelines. The Committee provided direction to staff as follows:

- 1) Expand the boundary for the Todos Santos Design Guidelines to include the following areas within the inner core: a) the Downtown Pedestrian zoned area on the north side of Pacheco Street; b) the eastern portion of the blocks between Colfax and East Streets (between Pacheco and Concord Blvd.; c) an area between Colfax St. and Grant St. (between Concord Blvd. and Park St.); and d) a portion of Salvio Street between Mira Vista and the western edge of the boundary, as shown on Attachment 1.
- 2) Confirmed the draft outline that would be further detailed once the project is initiated.
- 3) Confirmed thresholds – All new buildings and renovations of \$10,000 or greater in exterior improvements would be subject to the Guidelines.

NOTE: Staff has had further discussion, and recommends clarifying that (1) the threshold for new buildings and renovations applies to only exterior façade improvements and (2) increasing the threshold amount to \$50,000 to avoid unintended consequences of owners not conducting repairs or general maintenance, such as painting, to avoid the guidelines.

- 4) Incentives – Committee recommended looking at fee waivers as a potential method to provide incentives which will be further studied during the project.
- 5) Process – Staff will initiate preparation of the design guidelines in November with a 9-month timeline to remain on a parallel path for coordination purposes with the Corridors Project,

which is studying and preparing streetscape designs and design guidelines for Oak Street, Grant Street and Salvio Street.

Discussion

The purpose of the Todos Santos Design Guidelines would be to create guidelines that support authenticity and flexibility and retain Concord’s historical context. Staff clarified that parcels outside of the inner or outer core would still be subject to the design guidelines adopted within the Downtown Specific Plan in June 2015, which are less prescriptive but provide general design guidance on massing, urban form, character, ground floor treatment, relationship of facades to the public realm, vehicular access and building design, including parking, servicing and private open space.

Boundary Designations for the Todos Santos Design Guidelines

Attachment 1 represents the “inner core” in red, based on the Committee’s comments at the last meeting. One alternative to Attachment 1 could simply be to extend the southern boundary to only include Grant Street, to recognize improvements associated with the Corridors Streetscape project, and reduce the chance to have two very different styles of architecture along the two block street segment (Attachment 2).

On November 2, 2015, the City’s Ad-Hoc Committee met to hear progress to date on the Corridors Project and to view some early concepts in terms of public street furniture and related facilities. In addition, a public meeting was held on the project that evening. Staff believes it is important that the Corridors Project be closely coordinated with the Todos Santos Design Guidelines in developing conceptual designs for streetscape and intersection along the three corridors. Design guidelines for lighting and street furniture, storm water facilities, provisions for pop-up/temporary commercial uses and accessibility will also be prepared.

Next Steps

Staff is looking for input and guidance from the Committee as to their preference on the “inner” boundary for incorporation into the Design Guidelines. Staff would then initiate the preparation of the Todos Santos Design Guidelines and return to Council at an appropriate milestone, approximately 25% through the project, for a status update and further input by the Council.

Public Contact

The agenda item was posted. Developers with an interest in the downtown have been notified.

Staff Recommendation

Staff recommends the boundary delineated in Attachment 2. The boundary would recognize the current work and planned improvements associated with the Corridors Streetscape project, and reduce the chance to have two different styles of architecture along the two block street segment. In addition, staff recommends clarifying that (1) the threshold for new buildings and renovations applies to exterior façade improvements and (2) increasing the threshold amount to \$50,000.

DISCUSSION OF TODOS SANTOS DESIGN GUIDELINES FOR THE DOWNTOWN

November 16, 2015

Page 3



Jovan Grogan
Deputy City Manager

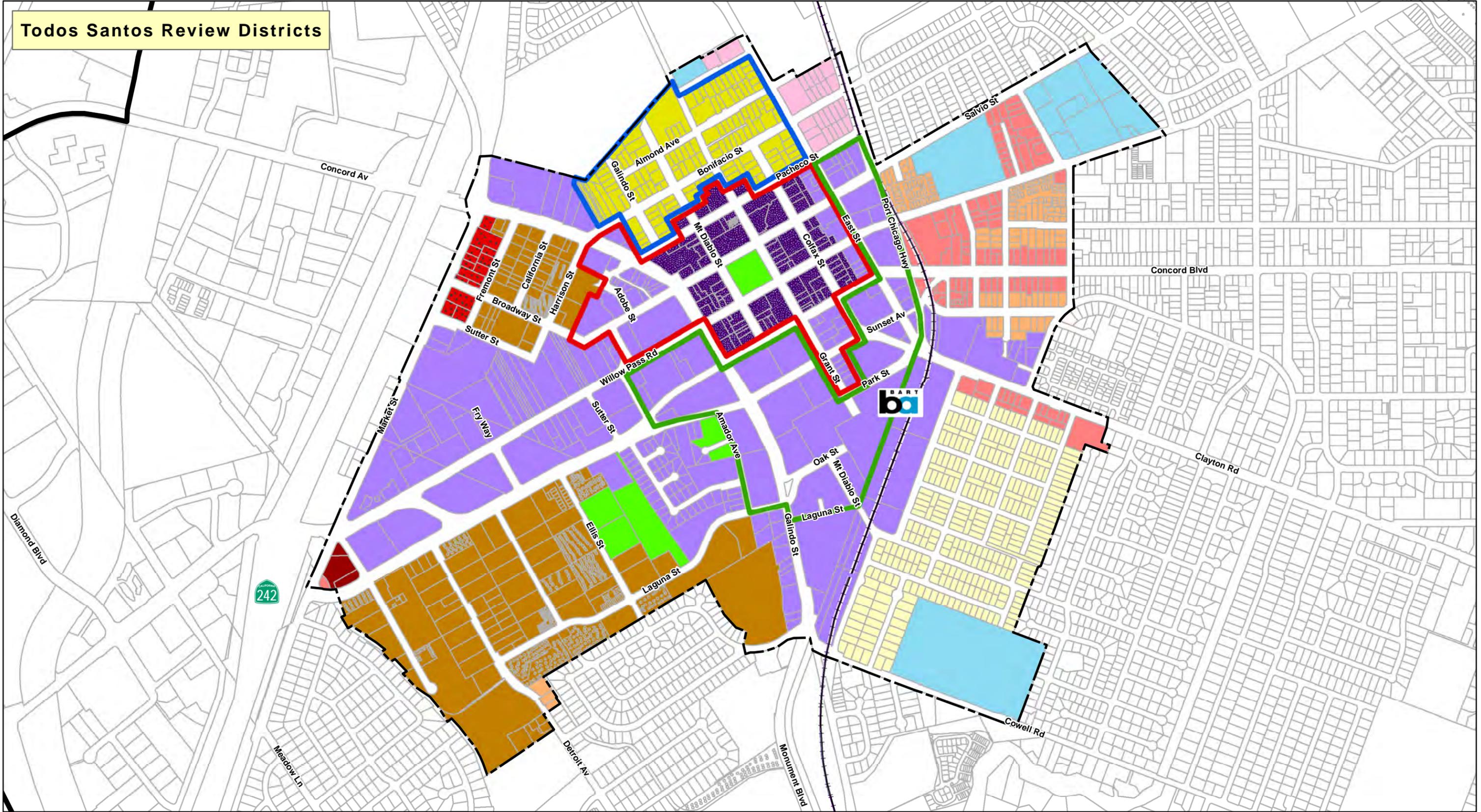
Jovan.grogan@cityofconcord.org

Prepared by: Joan Ryan, AICP
Senior Planner
joan.ryan@cityofconcord.org

Reviewed by: Laura Simpson
Planning Manager
Laura.simpson@cityofconcord.org

- Attachment 1 – Boundary designation map for Todos Santo Design Guidelines (Alternative 1)
Attachment 2 – Alternative 2 for Inner Core

Todos Santos Review Districts



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Legend

- Priority Development Area
- City Limit
- BART Station

Todos Santos Review Districts

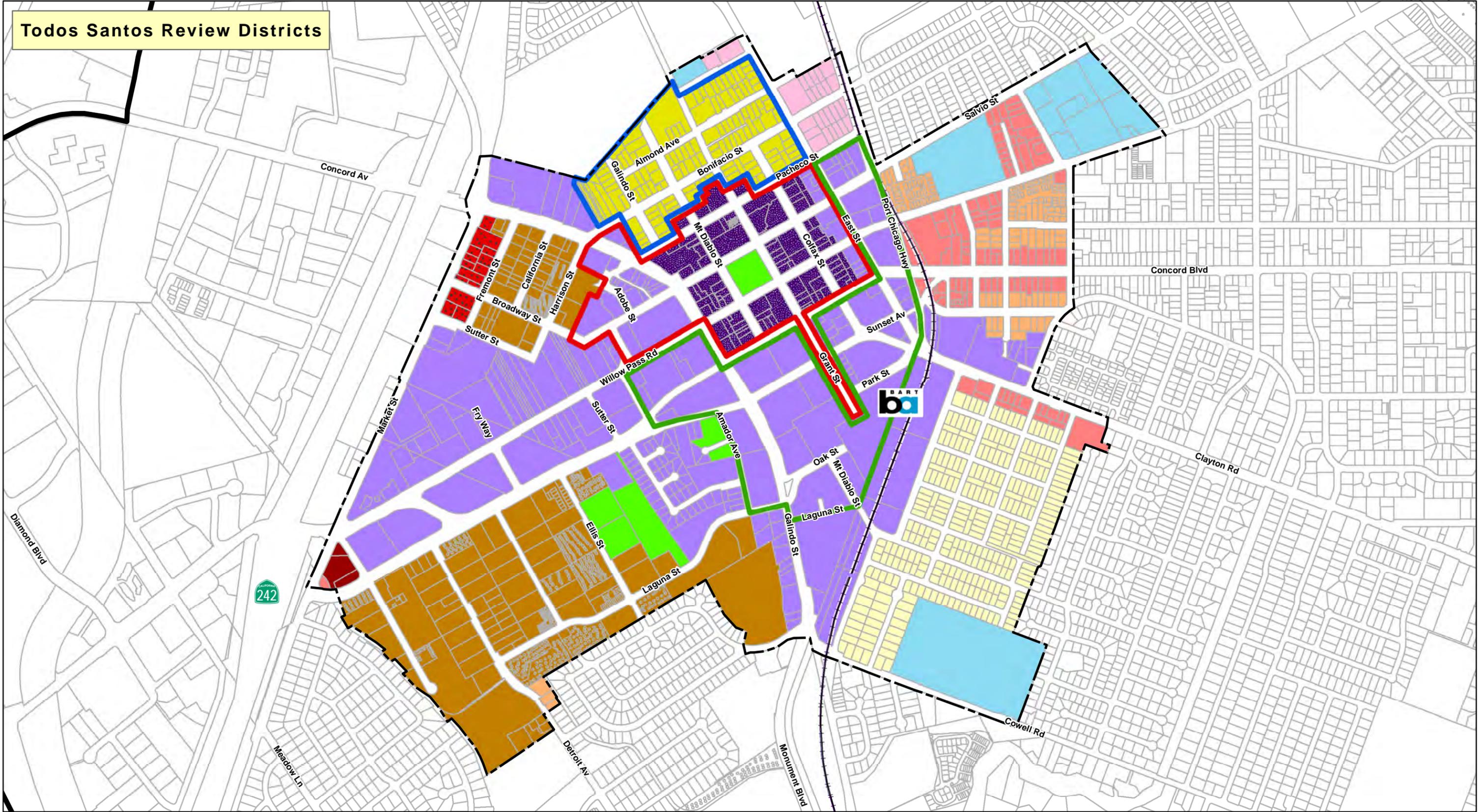
- Inner Core
- Outer Core
- North Todos Santos

General Plan 2030 Land Use

- Low Density Residential (LDR)
- Medium Density Residential (MDR)
- High Density Residential (HDR)
- North Todos Santos (NTS)
- Community Office (CO)
- Commercial Mixed Use (CMU)
- Regional Commerical (RC)
- Service Commerical (SC)
- Downtown Pedestrian (DTPD)
- Downtown Mixed Use (DTMU)
- Public/Quasi-Public (PQP)
- Parks and Recreation (P)



Todos Santos Review Districts



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Legend

- Priority Development Area
- City Limit
- BART Station

Todos Santos Review Districts

- Inner Core
- Outer Core
- North Todos Santos

General Plan 2030 Land Use

- Low Density Residential (LDR)
- Medium Density Residential (MDR)
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- Service Commerical (SC)
- Downtown Pedestrian (DTPD)
- Downtown Mixed Use (DTMU)
- Public/Quasi-Public (PQP)
- Parks and Recreation (P)



CITY COUNCIL COMMITTEE

SPECIAL MEETING

HOUSING & ECONOMIC DEVELOPMENT

Dan Helix, Chair

Ron Leone, Committee Member

5:30 p.m., Monday, November 16, 2015

**Building A, Garden Conference Room
1950 Parkside Drive, Concord**

- ANNOTATED AGENDA -

ROLL CALL: Committee member Ron Leone was present. Mayor Tim Grayson substituted for Committee Chair Dan Helix who was absent.

STAFF PRESENT: Victoria Walker, Director of Community & Economic Development; Joan Ryan, Senior Planner

MEMBERS OF THE PUBLIC WHO ADDRESSED THE COMMITTEE: None

PUBLIC COMMENT PERIOD: None

1. **DISCUSSION** – of Todos Santos Design Guidelines. Report by Joan Ryan, Senior Planner.

ACTION: Joan Ryan presented a brief update sharing the background from the most recent October 26th meeting. She then presented the two alternatives reflecting modifications per the prior input of the Committee. The two alternatives were similar, except for the area along Grant Street, where Alternative 1 included properties on the east side of Grant Street within the inner core and Alternative 2 did not. Ms. Ryan also updated the Committee on clarifications regarding the thresholds being tied to façade improvements specifically and increasing the amount to \$50,000.

The Committee discussed staff's recommendations. They agreed on Alternative 2, noting that it seemed appropriate that the properties on the east side of Grant Street be within the outer core. The Committee also agreed that the \$50,000 threshold seemed more appropriate along

CITY COUNCIL COMMITTEE

with tying the language specifically to façade improvements. They also requested that the Todos Santos Design Guidelines include some exclusion language and an appeal process for administrative review.

2. **ADJOURNMENT** at 6:10 p.m.

Reviewed by:



Jovan Grogan, Deputy City Manager

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Brian Libow, Interim City Attorney
Victoria Walker, Community & Economic Development Director
Laura Simpson, Planning Manager
Joan Ryan, Senior Planner
Administrative Services